

# Bangor Wharf



Report to accompany planning application:

Design & Access Statement incorporating Landscape Report  
TM Architects & Turkington Martin

February 2017

Bangor Wharf, Georgiana Street, London NW1 0QS  
Design & Access Statement

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Ref: 194\_DAS\_170227



# Bangor Wharf Georgiana Street, London NW1 0QS

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## Client

One Housing Group

## Design Team

|                      |                    |
|----------------------|--------------------|
| Planning Consultant: | Spenthorpe         |
| Affordable Housing   | GL Hearn           |
| Air Quality          | Mayer Brown        |
| Arboriculture        | Broad Oak          |
| Architect:           | TM Architects      |
| Ecology              | Wardell Armstrong  |
| Flood Risk           | Conisbee           |
| Ground investigation | GEA                |
| Heritage Consultant  | KH Heritage        |
| Landscape Architect  | Turkington Martin  |
| Noise Consultant     | Sharps Redmore     |
| SCI                  | Curtin & Co        |
| Structural Engineer  | Conisbee           |
| Sunlight & Daylight  | CHP                |
| Sustainability       | Couch Perry Wilkes |
| Transport            | Vectos             |
| Viability Consultant | BNP Paribas        |

## 1.00 Introduction

TM Architects and Turkington Martin have been appointed by One Housing Group to design a residential-led, mixed use scheme for the redundant site at Bangor Wharf, Camden Town.

This Design & Access Statement has been prepared to support the full planning application for: *Demolition of all buildings on-site and new buildings of 1-6 storeys in height to include 40 residential (C3) units (16 x 1 bed, 15 x 2 bed and 9 x 3 bed) of which 34 would be market units and 6 affordable, 813 sq.m (GEA) of new office floorspace (B1a), 55 sq.m (GEA) storage and distribution floorspace (B8) and associated works to highways and landscaping.*

One Housing Group is a leading developer of high quality homes for private sale, shared ownership and affordable rent which currently has an ambitious program to deliver 3,000 new homes by 2019. One Housing Group manages over 15,000 homes across 27 London boroughs and surrounding counties as well as providing care and support for over 11,500 people to help them live more independently.

One Housing Group is well-known to the LB Camden where it is based. One Housing Group is the affordable housing partner at Kings Cross and has already completed some of the new housing at Saxon Court and the Plimsoll Building (right, top and centre).

TM Architects have a long track record of working with One Housing Group and recently worked together on the design of a new mixed-use scheme for the redevelopment of their building at 100 Chalk Farm Road. The scheme received planning permission from Camden in 2014 (bottom right).

TM Architects have a broad portfolio of award-winning work including the development at



Top: Saxon Court, Kings X (Macreanor Lavington Architects)  
Centre: Plimsoll Building, Kings X (David Morley Architects)  
Bottom left: Kingsland Wharves, Hackney  
Bottom right: 100 Chalk Farm Road, NW1

## 2.00 Site location and context

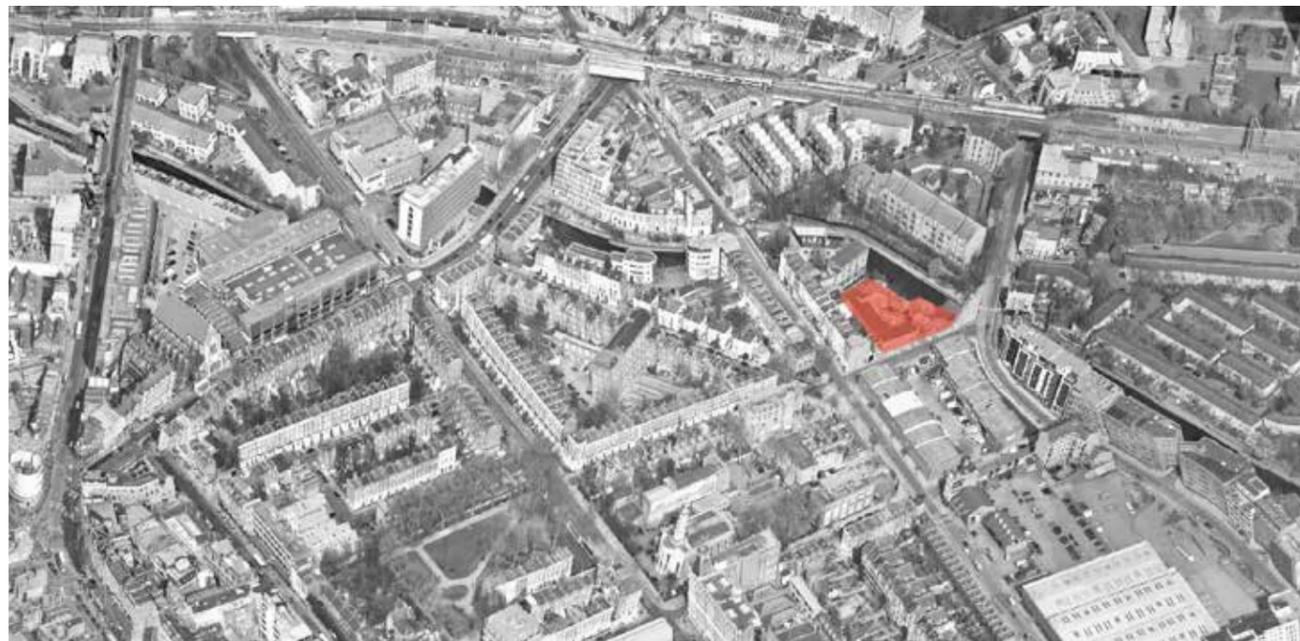
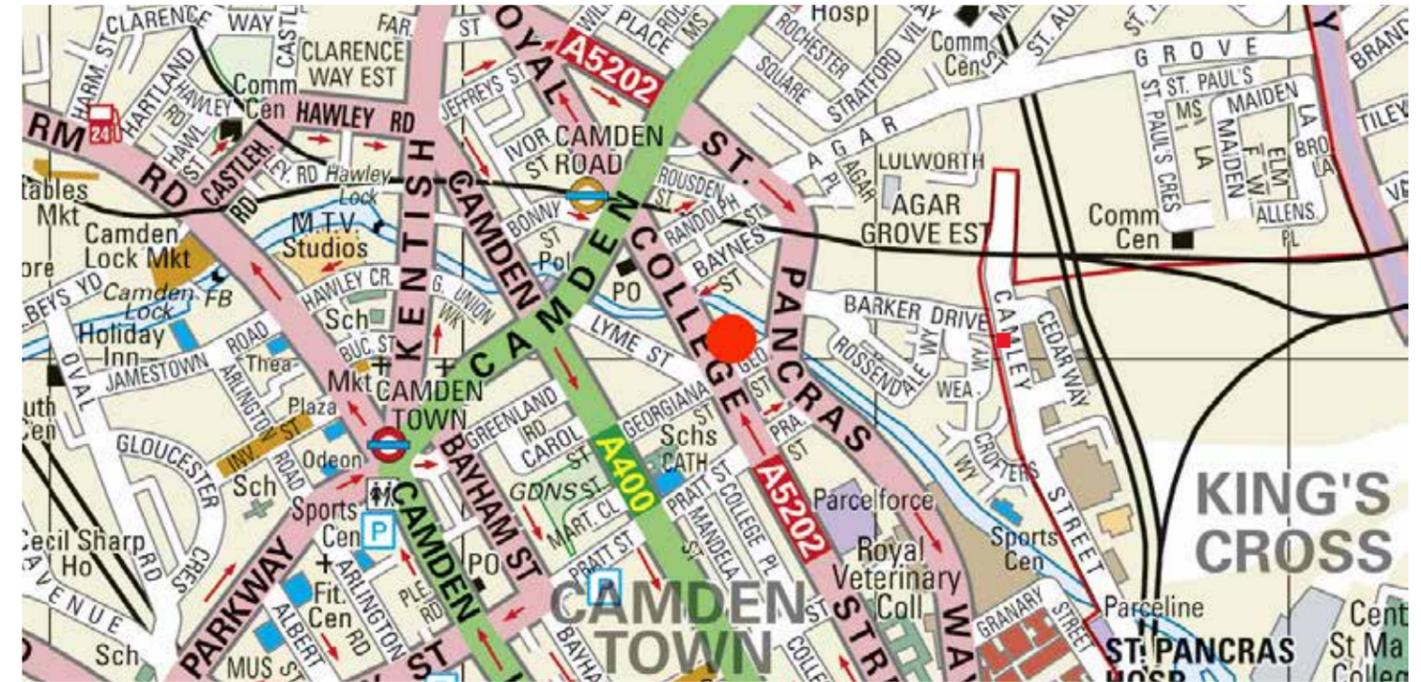
### 2.01 Site location

The site is in the London Borough of Camden, north-west of Kings Cross and east of Camden Town centre, adjacent to the Regents Canal. Camden High Street, Camden Lock and Market are a short walk away.

The site lies to the west of Kings Cross Central, also known as the Kings Cross Opportunity Area. This is one of the most significant sites of development and regeneration in Central London. The area covers 54 hectares of land and contains many buildings of architectural or historic interest and other features such as Camley Street Natural Park. The northern extent of the Opportunity Area is bounded by London Overground and the western extent by the railway lines from St Pancras. The Regent's Canal runs west to east through the middle. The area is due to include 50 new buildings, provide 2,000 new homes, 20 new streets, 10 new public squares and 26 acres of open space.

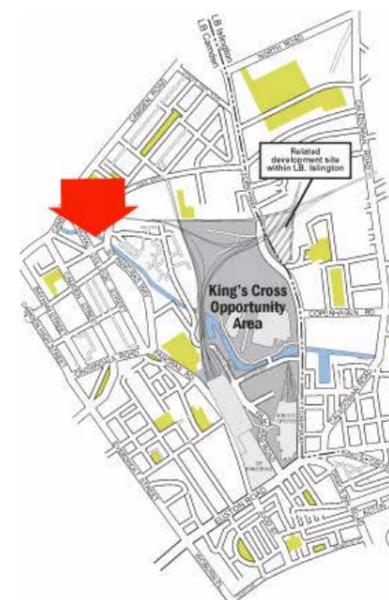
On the other side of the London Overground line to the north east is Agar Grove Estate, which is undergoing a major redevelopment by Camden Council, including demolition of all but the central tower and providing 513 new homes.

To the west and south-west of the site are Primrose Hill and Regents Park and to the north-west are Parliament Hill and Hampstead Heath. The West End is a short bus or tube ride away.



Above: aerial view of Camden Town, looking north with site indicated in red; right: Agar Grove Estate aerial view of estate as proposed following redevelopment (canal visible in top right corner)

Far right, clockwise from top left: Camden Lock, aerial view of Kings X Opportunity Area looking west with canal in centre and site indicated; Granary Square; Tapestry Building and Gasholder Park, Kings Cross; map of Kings X Opportunity Area with site indicated





Aerial view of the site in its context

## 2.02 Site history

The historic map dated 1827 (right, top) shows the Regents Canal, Fleet River and new streets laid out in this part of Camden Town. The triangular site that is to become Eagle Wharf and Bangor Wharf can be seen on the north side of Parliament Street (now Georgiana Street), complete with a dock. The River Fleet crosses the south-western corner of the site. This was later enclosed with a brick sewer.

Other maps show Bangor Wharf had been developed by the 1860's with a single building along the street frontage. By the mid-1870's some buildings appear on the waterside of Eagle Wharf. Insurance plans from 1890's show stables and a glue factory on the site. It is understood it was part of the Vestry of St Pancras refuse depot until the early 1930s. The site became an LEB depot during the 1960's, with EDF having used it most recently. The site is now vacant.

An earlier Grays Inn Bridge was replaced with the existing one in 1897, widening the junction of Georgiana Street and St Pancras Way. The photograph (below right) shows a building at Bangor Wharf fronting Georgiana Street on the left hand side of the bridge with its solid balustrading, later replaced with the railings seen in the photo below. Extensive bomb damage during WW2 resulted in demolition and construction of the current buildings on the site during the 1950's. The old dock was infilled around the same time.

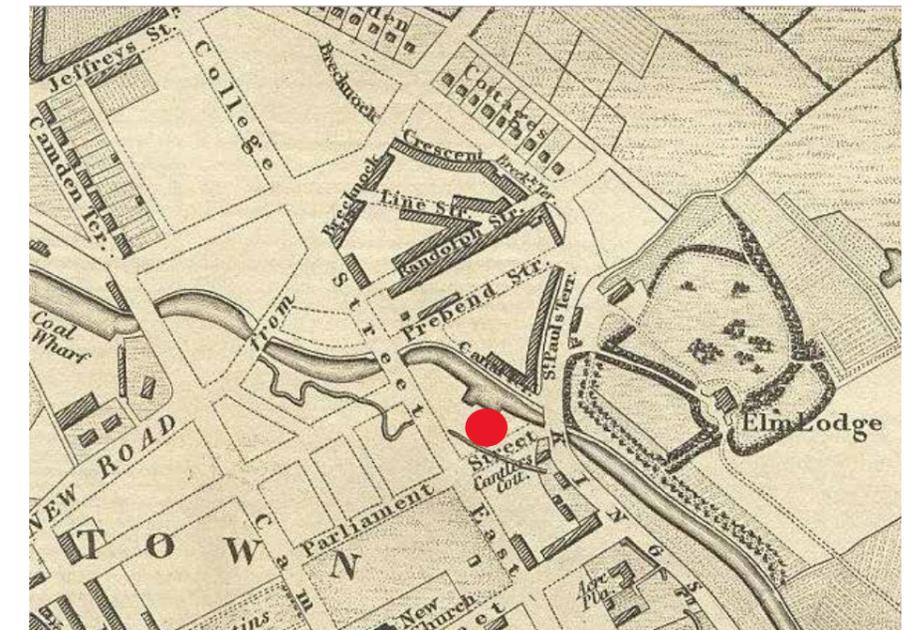
For further information, refer to the Heritage Statement.

Below left: photo of east side of Grays Inn Bridge from the tow path; railings replaced the larger, original solid panels visible in photo, right

Below right commemoration stone on Grays Inn Bridge, rebuilt in 1897



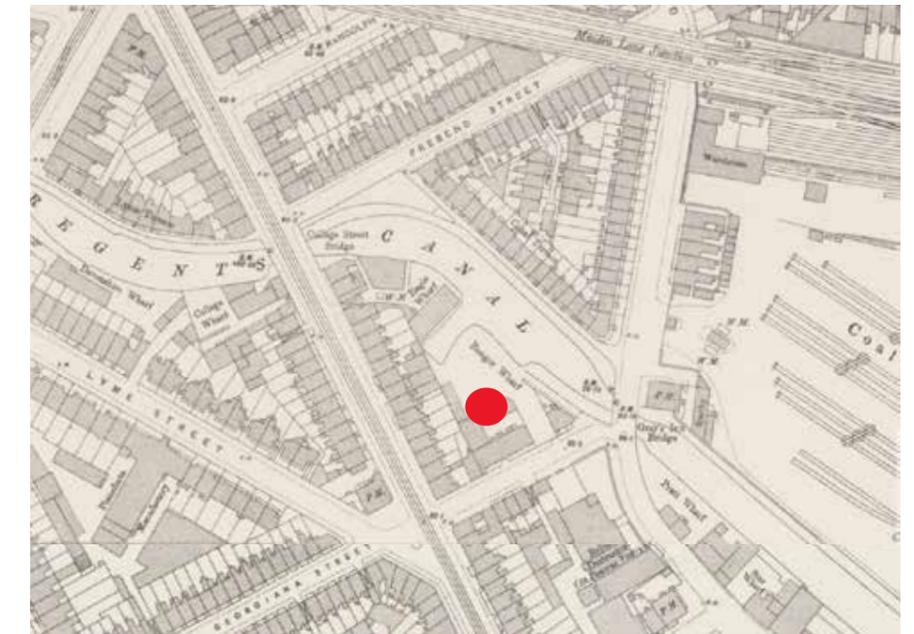
Above: photograph c 1947 looking south east through a hoarding from Prebend Street (now Baynes Street) towards Grays Inn Bridge with Canal Terrace (now Reachview Close) and The Constitution PH



Historic maps: Greenwood Map 1827 (above) Ordnance Survey Map 1893-96 (below)



Above: photograph c 1947 of Grays Inn Bridge from Kings Road (now St Pancras Way) looking north towards Canal Terrace (now Reachview Close) with one of the earlier Bangor Wharf buildings just visible, left

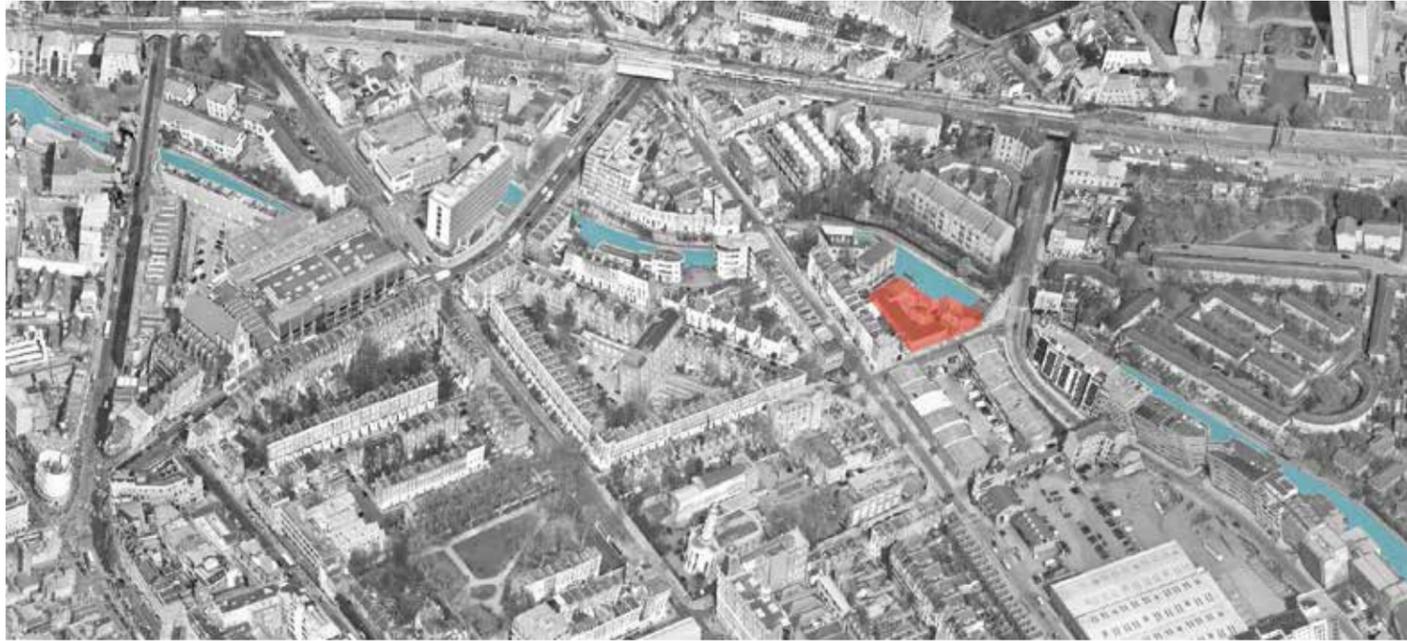


Photos, left: Fleet sewer which runs under the site; existing cobbled ramp on site, existing tunnel under the road

Right: former dock just visible between Eagle and Bangor Wharfs, infilled in 1950s



## 2.03 Context and analysis



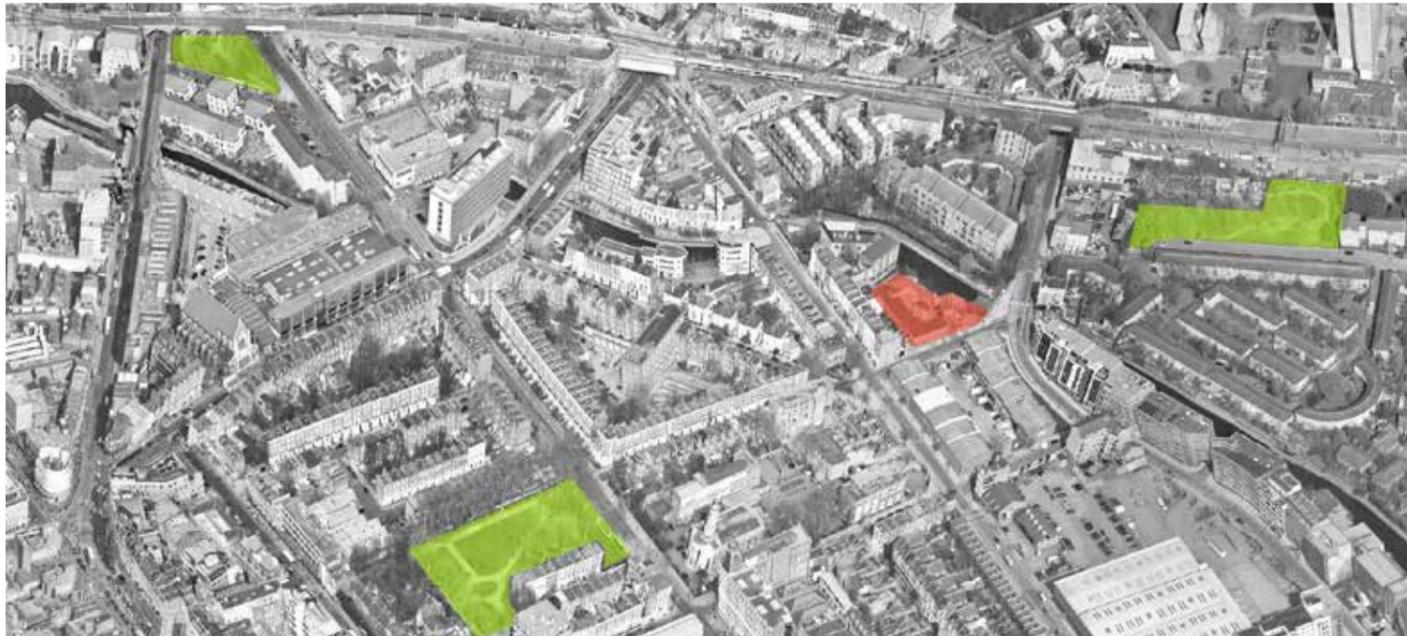
2.03a The Regents Canal (above)

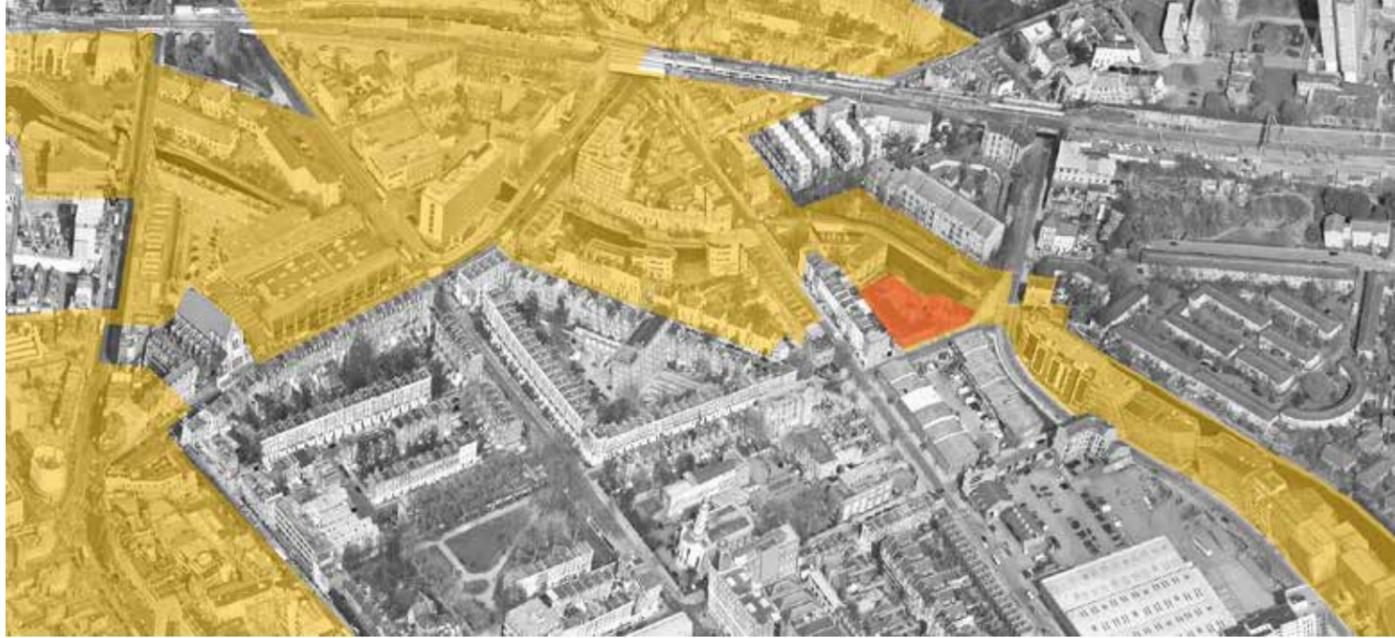
The principal physical context for the site is the Regents Canal which wound its way through this part of Camden Town to cope with varying topography and land ownerships at the time of construction. This means that there are no long views of the site when approaching from the west and Grays Inn Bridge cuts across the view when approaching from the east (photo, above right).



2.03b Green open space (below)

There are three small parks within 0.5km of the site. The closest is at Elm Village, with its children's play area, general park and horticultural project at the top of the steps adjoining the railway (photo below right). St Martins Gardens is a long-established park (photo below left) which also contains a children's play area. Slightly further afield is Camden Gardens.





**2.03c Conservation Areas (above)**

The site is located within the Regents Canal Conservation Area, as are the embankment on the opposite side of the canal, The Constitution PH, the buildings to the east of Grays Inn Bridge, Lyme Terrace and Lawfords Wharf to the north-west. The Regents Canal CA is immediately adjacent to Camden Town Conservation Area and Jeffreys Street Conservation Area, all of which are shown above.



**2.03d Listed buildings (below)**

There are no statutorily listed buildings adjacent to the site but there are a large number nearby, mostly terraces of nineteenth century houses. The nearest is the terrace on the west side of Royal College Street (photo left below). Lymehouse Studios (below right) is a Grade II listed building in Lyme Street.



2.03 Context and analysis (continued)



Land use:

-  Use Class A: retail, restaurants, PH's etc
-  Use Class B: employment
-  Use Class C: residential
-  Use Class D: schools, places of worship, community centres etc



2.03e Building uses (above)

The area is mixed use, but with residential as the predominant use.

2.03f Building heights (below)

The local area has a mix of building heights, from single storey to 7 storeys. To the north-east of the site is an 18-storey tower on the Agar Grove Estate. Buildings around Camden Road station are 7 storey (below left and right), with planning permission granted for another 7 storey building on the corner of Camden Street and the canal.



Building height:

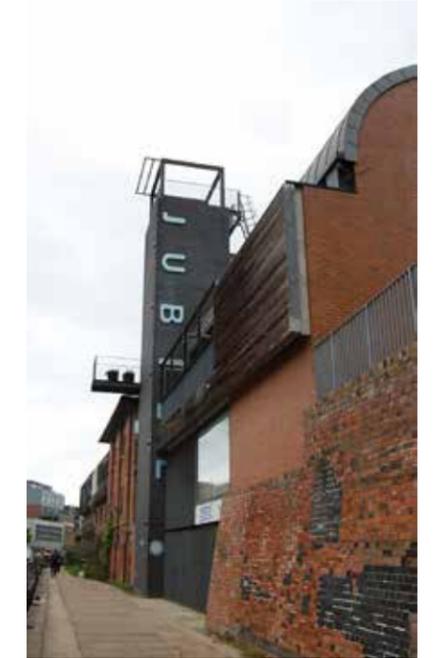
-  1 storey
-  2 storey
-  3 storey
-  4 storey
-  5 storey
-  6 storey
-  7 storey
-  18 storey





Character areas:

- 1 Regent's Canal
- 2 Victorian streets
- 3 modern housing
- 4 light industry
- 5 modern mixed use
- 6 High Street



2.03g Character areas (above)

The area includes a number of distinct character areas. the site sits at the junction of 4 of these:



## 2.04 Immediate context

There are no statutorily buildings immediately surrounding the site. 120-136 and 140-142 Royal College Street are locally listed, but are described as very degraded.

There are one or two buildings that are listed in the Conservation Area Appraisal as 'Buildings and other structures which make a positive contribution'. The following are listed as such in the immediate surroundings:

- 148 and 150 Royal College Street
- Former forage warehouse at Eagle Wharf and former dock
- Grays Inn Bridge
- The Constitution Pub at 42 St Pancras Way

This page: The Constitution PH and Grays Inn Bridge (top), existing mixed use terraces on Royal College Street and light industrial units on Royal College Street/Georgiana Street (centre), the Regents Canal south of Grays Inn Bridge (bottom)





Eagle Wharf 146 & 150 Royal College Street, the Regents Canal and Reachview Close

Above & left: Eagle Wharf (146 & 150 Royal College Street) and Regents Canal  
Below centre & right: Reachview Close



## 2.05 Wider Regents Canal context

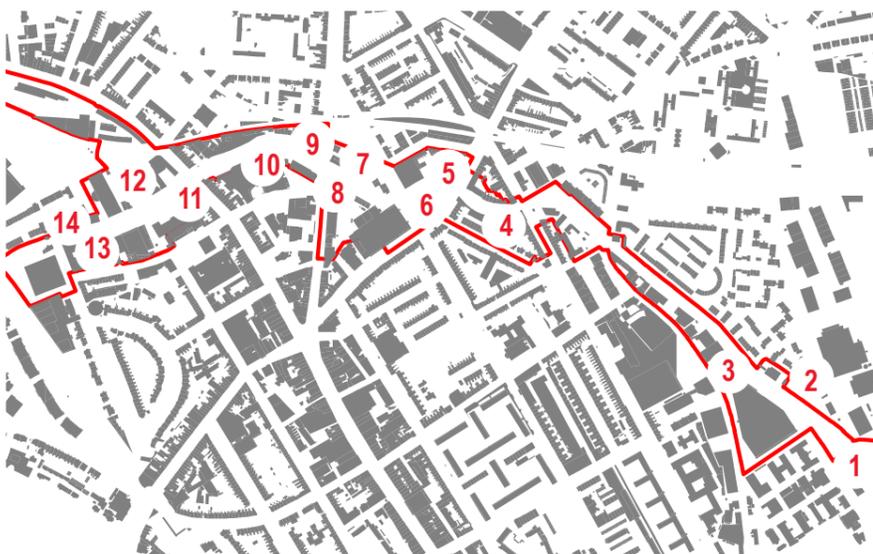
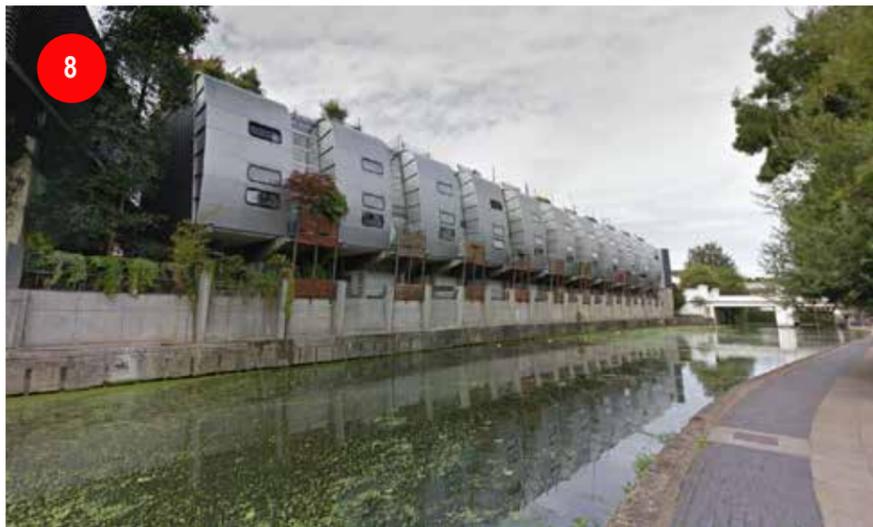
The site is a former wharf on the Regents Canal and, although the buildings currently on the site have no relationship with the waterway, nor any connection to its industrial heritage, there are a number of examples of retained, traditional warehouse buildings along the stretch of the Canal between Camden Lock and Kings Cross. There are also more recent buildings, some of which try to reflect the characteristics of the older buildings and some of which are a total contrast. The differing architectural idioms, uses and scales contribute to the character of this part of the Canal. And within the Kings Cross Opportunity Area, there are a series of new buildings, such as those shown below at Camley Street, which are currently under construction and which will further affect the character and backdrop to this site and the wider area.

### Key to photos

- 1 Fish & Coal Building, Kings Cross Central: currently undergoing conversion David Morley Architects (2-3 storeys)
- 2 Aerial view of Urbanest and other development at Camley Street: AHMM and KSR Architects (7-13 storeys)
- 3 Canal Reach building & Canal Side Studios, St Pancras Way (4-6 storeys)
- 4 Lawfords Wharf, Lyme Street: John Thompson & Partners and listed cottages (2-5 storeys)
- 5 Regents Wharf, Camden Road: Squire & Partners (4-8 storeys)
- 6 Transport Police HQ, Camden Road (8 storeys)
- 7 Camden Gardens: Jestico & Whiles (3-5 storeys)

- 8 Grand Union Walk, Kentish Town Road: Grimshaw Architects (3 storeys)
- 9 Hawley Wharf, Kentish Town Road: AHMM (3-9 storeys) - under construction
- 10 MTV (formerly TVam Building): Terry Farrell & Ptnrs (2-3 storeys)
- 11 Ice Wharf & Lock Keepers house, Camden Lock (1-4 storeys)
- 12 Interchange Warehouse, Camden Lock (5 storeys)
- 13 Gilbey House (formerly gin factory): Mendelsohn & Chermayeff (7-8 storeys)
- 14 Henson Building, Oval Road: conversion by Tate Hindle Architects (6 storeys)





## 2.06 Planning policy background

### 2.06a Use

The site was last used as a depot by EDF comprising employment B1 (offices 850m<sup>2</sup>) and B8 (storage 226m<sup>2</sup>). Historically the site was used for stabling of horses and storage in connection with the canal. It was then used by St Pancras Borough Council as offices, workshops and storage. In the early 'sixties the site was taken over by the LEB as a depot and more recently was used by EDF. The site is now vacant.

### 2.06b Site Allocations Local Development Document

The site is included in the Council's Site Allocations document (adopted September 2013) as Site 35 (see right). It says any redevelopment should provide replacement employment floorspace and new permanent Class C3 residential accommodation.

It provides 8 bullet points outlining the Council's specific expectations for the site and lists the relevant Core Strategy policies that apply. These include optimising the potential of the site to provide new housing, providing flexible space for a range of employment uses, providing an active frontage to Georgiana Street and looking for opportunities to link to the towpath (which doesn't apply here as it runs along the other bank), be of a form and scale that is appropriate to the CA and respond to the open character of this part of the canal and contribute to biodiversity and the green nature of the canal.

Under Main Policy Considerations, the guidance states that: '...The site is close to the Kings Cross development area and so will be influenced by development here (Policy CS4).' We note that the KX effect has already been felt further along this stretch of the canal where the new Urbanest building has been built and two more tall buildings are under construction. This cluster of tall buildings is significantly changing the character of this part of the Conservation Area and further change should be anticipated.

Site 35 guidance also states '...Any additional development on the site would be expected to include new homes....and to make the most of the site's capacity for housing

The guidance advises that existing buildings and boundary treatments detract from the character of the CA and there are opportunities for enhancement.

It says the character of this part of the canal is fairly open and that any development should avoid excessive bulk and massing along the canal. It also identifies opportunities for opening up views of the canal on the south-eastern corner of the site.

#### Site 35: Bangor Wharf, Georgiana Street



**Site Details**  
 Area: 1,810 sq m  
 Ownership: Canal Securities  
 Existing Use: Depot for storage of materials with ancillary workshop and offices (Class B8)  
 Ward: St Pancras and Somers Town

#### Planning Context

**Planning Designations**  
 Conservation Area: Regents Canal

#### Other Information

The Regents Canal runs adjacent to the site.  
 Public transport accessibility level: 6a  
**Relevant Planning Applications**  
 Sept 1999 Certificate of Lawful Use refused for existing use for builders workshop, office and open yard area (PEX990072)  
 August 2000 Certificate of Lawful Use granted for an existing use as a depot for storage of materials with ancillary workshops and offices (Class B8) (PEX0000739)

#### Site allocation guidance

**Redevelopment of the site to provide replacement employment floorspace and new permanent (Class C3) residential accommodation**  
 Development will be expected to:

- Optimise the potential of the site to provide new housing (including affordable housing) while minimising potential conflicts between residential and other uses
- Provide flexible space suitable for a range of employment uses
- Contain an active frontage to Georgiana Street, and to maximise opportunities to

- provide linkages to the canal towpath.
- Be of a form and scale which is appropriate to the Regents Canal Conservation Area and responds to the open character of this part of the canal and to surrounding listed buildings
- Take opportunities to utilise the canal for the transportation of goods and materials, both during construction and in the operation of the development
- Ensure that the design and layout of the development responds positively to its canal setting, and contributes to the biodiversity and green nature of the canal
- Provides active frontage to the canal and to Georgiana Street, in order to improve the relationship between the site and the public realm and to enhance the appearance and safety of the surrounding street scene
- Provide infrastructure for supporting local energy generation on site and/or connections to existing or future networks where feasible

#### Site Context

The site contains the offices, storage buildings, yard and vehicle parking for a small building construction company. It has good access to the strategic road network and unrestricted access for deliveries. Therefore it is considered to be a suitable site for continued employment use. However, the buildings on site are generally of lower density than those in the surrounding townscape so there does seem to be some potential for a more intensive development of the site involving other uses in addition to employment.

The site is within the Regents Canal conservation area. The current use, with its associated buildings and boundary treatment, detract from the character of the conservation area. It is identified in the Conservation Area Appraisal (2008, page 20) as a site with opportunities for enhancement. The yard area retains extensive areas of granite setts which should be exploited in any development. The canal dock which formerly served these wharfs is partially filled, and could be enhanced. The frontage onto Georgiana Street could be improved and should be addressed by any development.

The character of this section of the Regents Canal is fairly open so any development should avoid excessive bulk and massing along the canal and ensure that views of the canal are improved. There does seem to be some potential for opening up views of the canal on the south east corner of the site close to the Gray's Inn Bridge.

#### Main Policy Considerations

The site is close to the Kings Cross development area and so will be influenced by development here (Policy CS4). Development will be expected retain employment uses on the site unless it can be demonstrated that the site is no longer suitable for the existing business use, and that alternative business uses have been fully explored over a period of time (Policy DP13). The provision of new flexible workspace suitable for a range of employment uses such as creative industries and small and medium enterprises on the site would be encouraged as part of the provision of any new employment floorspace (Policy CS8).

Any additional development on the site would be expected to include new homes in line with policies CS6 and DP1, and to make the most of the site's capacity for housing (Policy DP2). In line with CS10 and DP15 development that increases the demand for community facilities and services would be expected to make appropriate contributions towards providing new facilities or towards improving existing facilities. Development will also be expected to make use of energy from efficient sources, including decentralised energy networks (Policy CS13) and should be of a scale and form which respects the character and appearance of the Regents Canal (Policy DP25).

#### Further Information

The main properties which would be affected by the redevelopment of this site are a row of terraced houses running from 116-132 Royal College Street to the west of the site. The majority of these properties have two to three storeys of residential accommodation with rear facing windows and back gardens which are about 10m deep although 128-134 appear to have complete site coverage

The site currently fails to respond to its canal setting, and fails to provide an active frontage to Georgiana Street. Any future development should make the most of opportunities to address this by providing more active frontages to both the canal and to Georgiana Street.

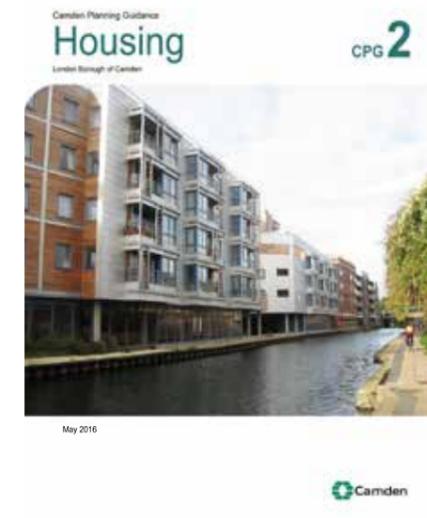
The current site includes a two storey office building which is located along the western boundary of the site close to the above properties. Any new development in the western portion of the site should consider these properties and not significantly worsen the relationship with these habitable room windows. This is likely to limit the height of development on this part of the site. When considering the scale, design and layout of the scheme, regard should also be given to the impact of any development on the setting of the Grade II listed three storey terraced houses to the west of the site at 16-31 and 32-53 Georgiana Street.

The existing use of the site as a storage depot and workshop means that there would be a need to investigate the site for contaminated land (via a preliminary assessment) prior to the submission of any planning application for redevelopment of the site.

This site and sites 40 and 41 below could contribute to a decentralised energy system. The sites are in close proximity to the community heating for social housing bounded by Plender Street and Camden Street, and we would expect the energy strategy for each development to test the feasibility of establishing a heat network to link with nearby Camden housing estates. Generally it would be expected that the largest site (Site 41-24-58 Royal College Street) would house the plant with the other sites contributing to provision of the infrastructure.

Camden Core Strategy  
 2010-2025  
 Local Development Framework

Camden Development Policies  
 2010-2025  
 Local Development Framework



## 2.06c Conservation Areas

The site falls within the Regents Canal Conservation Area. The 2008 Conservation Area appraisal document includes the site under Sub-Area 2: Kentish Town Bridge to the Oblique Bridge. It includes the following (although the yard mentioned below is covered in tarmac, with only a small number of granite setts remaining, and the former dock is fully infilled):

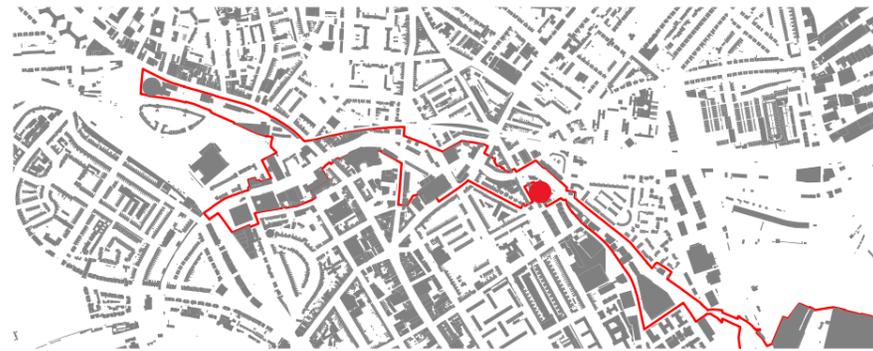
'Beyond College Street Bridge (Royal College Street) is one of the largest open planted sections to the canal, the steep bank rising up from the towpath with trees at the top of the bank forming a valuable visual containment. On the opposite bank is an excellent example of the reinstatement of a historic canalside warehouse building at Eagle Wharf, whilst the depot site adjacent at Bangor Wharf provides an excellent opportunity for enhancement. The latter's yard area retains extensive areas of granite setts which should be retained or re-used in any development. The canal dock which formerly served these wharfs is partially filled, and could be enhanced.'

The document provides the following advice on New Development within the CA:

'The conservation area is varied in scale and new design should respect the scale of the particular location. Appropriate design for the conservation area should complement the appearance, character and setting of the existing buildings and structures, the canal, and the environment as a whole. The enclosure or openness of particular sections of the canal should be respected as this quality contributes significantly to its varying character. Building heights should not interfere with views to local landmarks. Developments should respect and where possible enhance central London panoramas and other views from within and outside the conservation area.'

Under the Section entitled 'The Canal Side and the Public Realm', the document states:

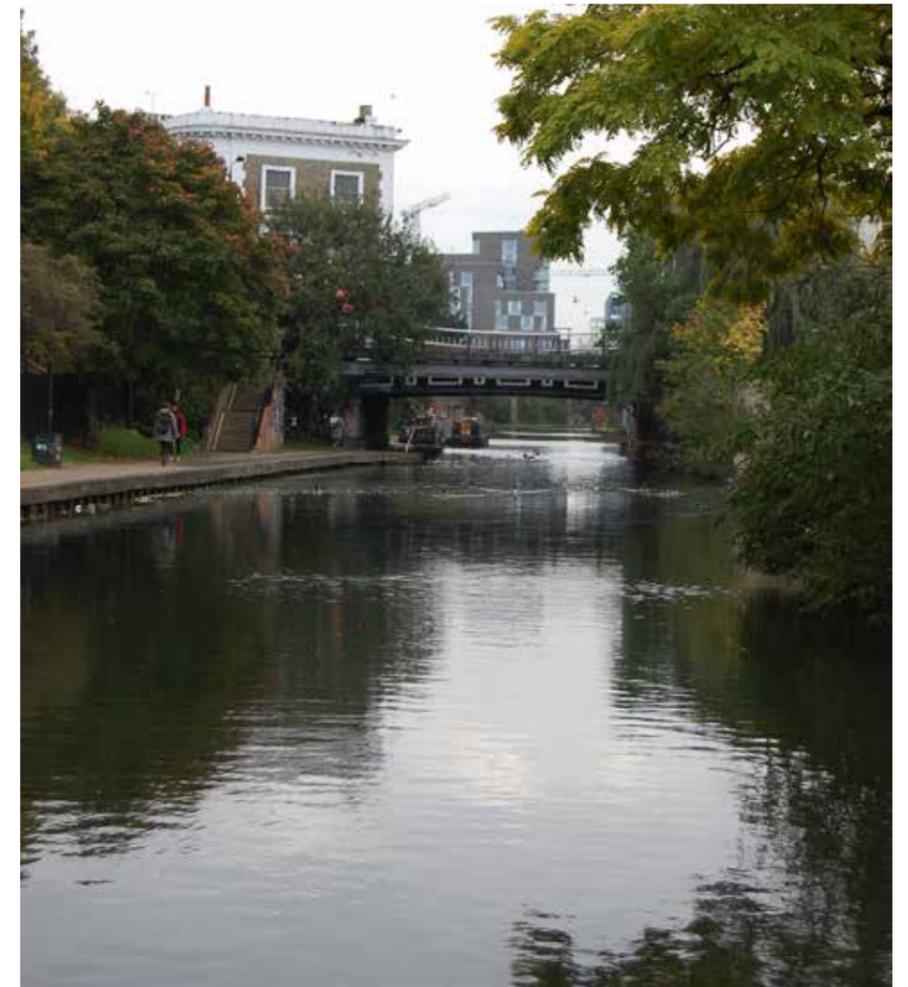
'The towpath generally runs along a single side of the canal. The opposite side of the canal, the offside, has buildings built directly onto the canal side in some instances and in others they are set slightly behind the canal edge. New development should respond to the character of the particular section of canal and in particular it's existing sense of enclosure or open aspect.'



Above: Regents Canal Conservation Area boundary



Above: view of Eagle Wharf and infilled former dock from opposite tow path  
Below left: view of 150 Royal College Street as seen from street level at College Street Bridge; below: view of Reachview Close from towpath



Above: view of Grays Inn Bridge from the towpath with The Constitution PH, left, and new development at the Oblique Bridge to the south, centre  
Below: view of Eagle Wharf from Grays Inn Bridge



Above: view of site from canal towpath



## 2.06 Planning background (continued)

### 2.06d Planning history

A previous planning application (ref 2016/1117/P) was made for the redevelopment of the site in February 2016 for: *Redevelopment of site to create a residential-led mixed-use development comprising 46 residential units (Use Class C3) (18 x 1 bed, 19 x 2 bed and 9 x 3 bed), new office floorspace (Use Class B1a) (686m<sup>2</sup>) with associated works to highways and landscaping following demolition of existing buildings.*

The application was refused in June 2016 on a number of grounds including loss of employment floor space, design, insufficient affordable housing and absence of a legal agreement.

Since then, the applicant and design team have held a series of pre-application meetings with planning officers and have attended a full Design Review Panel and subsequent Chair's Review in order to develop the detailed design of the proposed buildings.

Later in this Statement are notes regarding the various aspects of design that were discussed with officers and how these items evolved through review of options to the point where they were agreed and officers were able to support the application.



Previous planning application: canalside elevation as proposed (above); CGI view looking east along canal (below left); 3-d aerial view, looking south (below)



### 3.00 The site as existing

#### 3.01 Description

The site is triangular with its long, north-eastern side forming a boundary with the Regents Canal. The southern side of the site runs along Georgiana Street and the western side backs onto the rear boundary walls of houses/workshops in Royal College Street.

In the south-eastern corner of the site is Grays Inn Bridge, which has its abutment adjoining the site boundary. Adjacent to the abutment (but within the site) is a ramp leading down to a tunnel that runs under the road. Beneath that, running diagonally across the site, is the Fleet sewer.

The site was formerly used as a depot for EDF with offices, storage, parking and a sub-station. The buildings are modern and have no architectural or heritage merit. The 'extensive area of granite setts' referred to in the Site Allocations document have largely been removed, other than around the periphery and on the ramp.

The site is approx 0.18ha.



Above: existing Bangor Wharf 2-storey offices seen from inside courtyard  
Below left: view of yard from Georgiana Street entrance gates; centre: view of yard and B8 storage buildings running along canal; right: view of 1-storey building at n-e end of site with Eagle Wharf beyond



External views of the site as existing seen from Regents Canal towing path (top) and Georgiana Street (bottom)



Grays Inn Bridge  
(St Pancras Way)

Eagle Wharf (150 Royal  
College Street)



Georgiana Street/  
Royal College Street

No 54  
Georgiana Street

Grays Inn Bridge  
(St Pancras Way)

### 3.02 Site analysis

Bangor Wharf is a constrained, inner city, brownfield site. The main issues are highlighted in the diagram (right).

The site falls within the Regents Canal Conservation Area.

Although there are no statutorily listed buildings immediately adjacent to the site, there are some locally listed and a couple of buildings that make a contribution.

The Regents Canal runs along the north-eastern boundary of the site.

The Fleet sewer runs directly beneath the site and is close to the surface, restricting the options for building over and utilising the site's lower levels

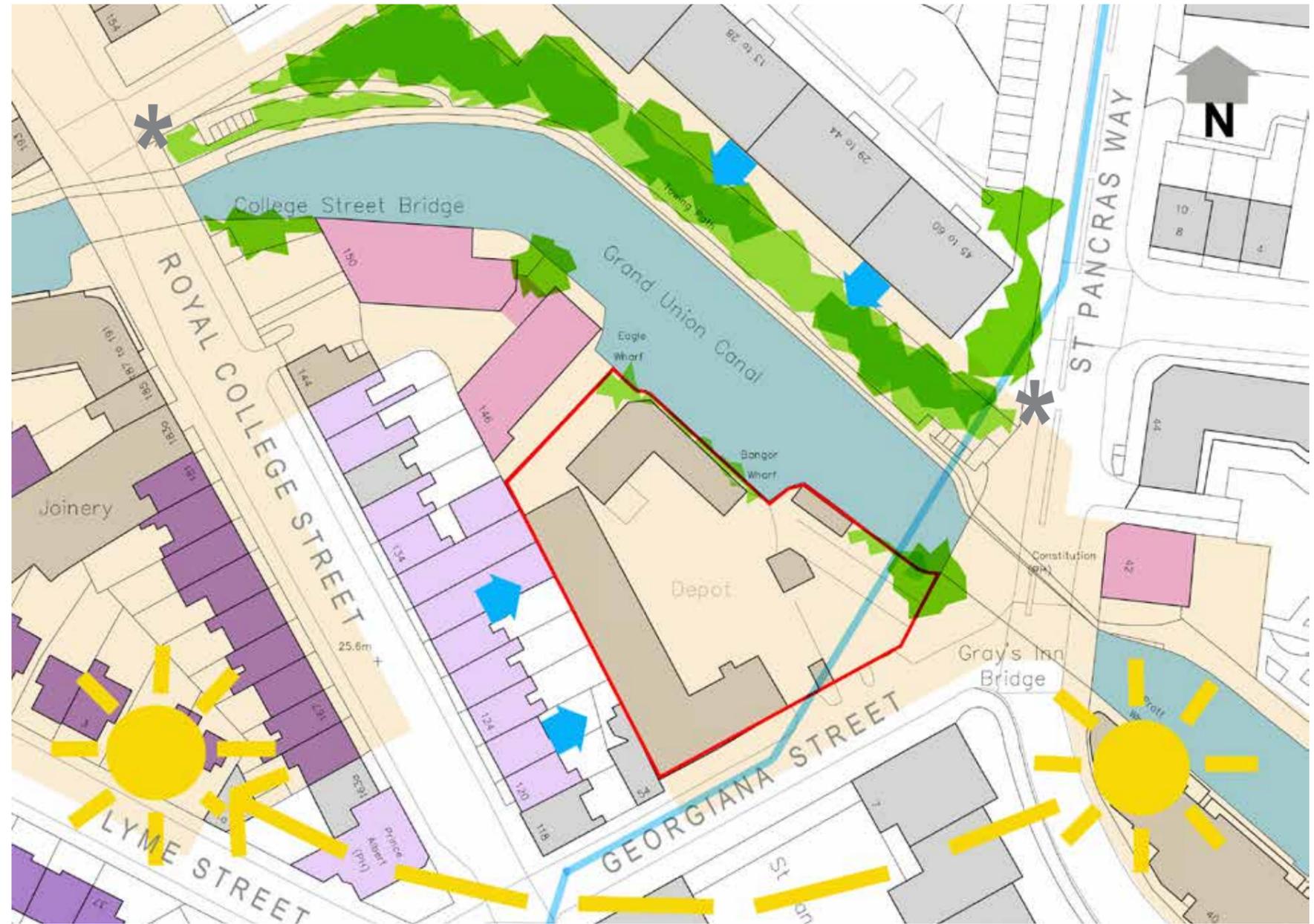
Grays Inn Bridge has one of its abutments on the south-eastern boundary, restricting the ability to build up to it. There is a cable tray that projects from the face of the bridge over the site, which needs clearance.

To the north of the site is Eagle Wharf, a Victorian warehouse building and former forage store. This is in commercial use and has windows in its south-eastern elevation.

The site is surrounded by a number of residential properties which have habitable rooms looking directly at it. The closest are in Royal College Street and any development proposals need to respect their privacy/overlooking and levels of daylighting.

There is no significant planting on the site, other than a mature weeping willow tree in the south-eastern corner.

The site is closed with no views of or access to the canal. There is currently an inactive frontage to the street



#### KEY

|   |   |   |   |   |   |
|---|---|---|---|---|---|
|  | Site boundary                             |  | Approx line of Fleet Sewer (Camden Road branch) |  | Regents Canal                             |
|  | Existing buildings                        |  | Windows to hab rooms in existing houses/flats   |  | Buildings that make positive contribution |
|  | Extent of Regents Canal Conservation Area |  | Existing trees                                  |  | Sun path                                  |
|  | Listed buildings (Grade II)               |  | Access point to canal towpath                   |   |   |
|  | Locally listed buildings                  |   |   |   |   |

### 3.03 Opportunities

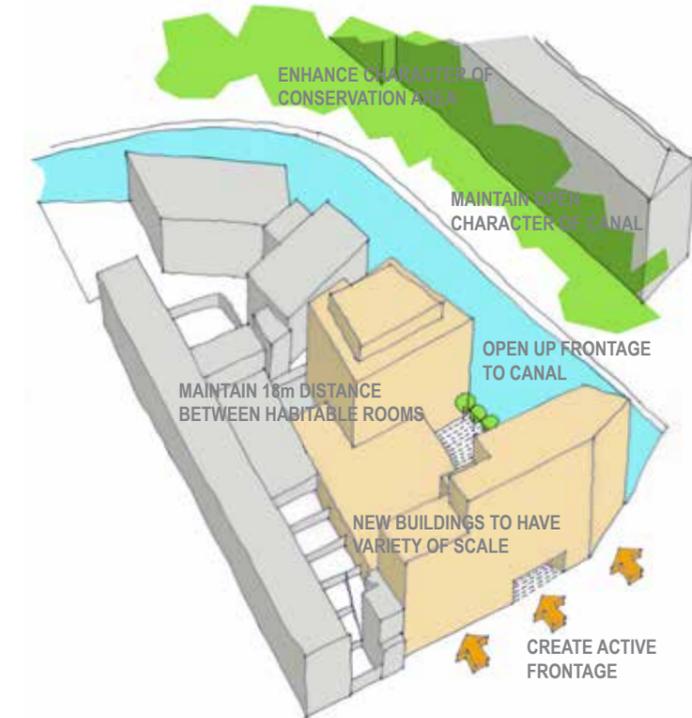
The proposed redevelopment of Bangor Wharf provides the opportunity to deliver a scheme that meets the majority of the aspirations included in the Site Allocation Policy described above.

Our approach to this is as follows:

- we recognise this is a special site within the Regents Canal Conservation Area, located between two bridges on a bend in the canal
- to take the opportunity to open up the canal frontage to allow views in/out
- to create a new central space as shared amenity/public space, open to the water with new buildings around it
- to pick up on variations in canal edge conditions to create interest in the layout of new buildings
- to take references from Eagle Wharf and other buildings along the Regents Canal of traditional warehouse architecture to inform the design of new buildings
- to develop new buildings of a scale that respects the context but also maximises the opportunity to provide new housing as well as better quality workspace
- to use the above to design a scheme that will enhance and improve the setting of the surrounding buildings of townscape interest and of the the Regents Canal Conservation Area



Early 3-d sketch of approach to design



### 3.04 Design development

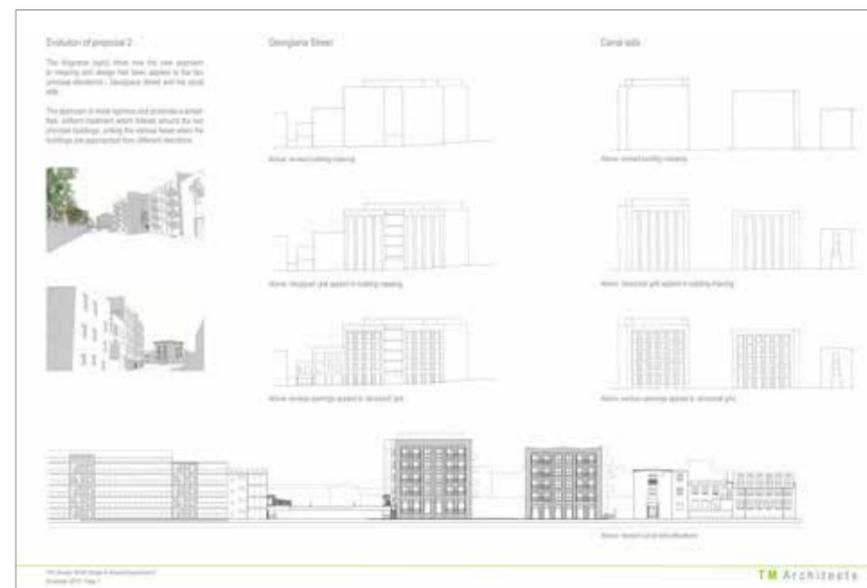
Following issue of the planning refusal in June 2016 (see Section 2.06d above), the design team held a number of pre-application meetings with Officers to develop the detailed design. There have been a total of six pre-application meetings/design workshops, a full Design Review Panel and a Design Panel Chair's Review.

During the previous application's determination period, Officers acknowledged the principles of the layout, height and massing were acceptable. In an email dated 7 April 2016, the Case Officer said '...the layout, scale and orientation of the proposed buildings are, in general terms, considered to be appropriate to the site and broadly supported.' However the delegated report dated 17 June includes the following under para 6.7 Height and massing: 'The proposed buildings would be materially greater in height, scale and massing than those within its immediate and surrounding context. Development of this level would only be suitable subject to an exemplary design using careful consideration of the characteristics of the site, features of local distinctiveness and the wider context in order to achieve high quality development which integrates into its surroundings, in accordance with policies DP24 and DP25'.

The pre-application meetings and design workshops have worked through the main issues in order to achieve the required exemplary design. These include the following:

- identification of the main characteristics of traditional warehouses to inform the detailed design
- inclusion of recessed panels in brickwork to 'express the structure'
- regularity of openings within these panels
- relationship of solid to void (brickwork to window openings)
- degree of active frontage and design of the main entrance
- building massing and treatment of setback top floor
- limited palette of traditional building materials and brickwork detailing
- balcony design and detailed treatment of recessed balconies on Georgiana Street

To accompany the pre-application meetings and design workshops, a series of design supplements were produced to summarise the progress of the design. Extracts from some of the documents are shown, right.



### 3.04 Design development (continued)

The proposals were also presented to the Camden Design Review Panel in August 2016 (see extracts on previous page) and again to a Chair's Review in December. The main issue raised was proposed scale of buildings, which the Chair felt was too high and should be reduced to a maximum of 4 storeys. This was looked at by the design team but rejected as it would result in very little employment space and no affordable housing. Officers reiterated their support for the scale as long as the detailed design was worked through. Otherwise the panel were generally supportive of the design approach.

At the second presentation the Chair acknowledged improvements to the design (although still felt some parts of the proposal were too tall) and concluded they could support a new application.

Computer models were used throughout to inform the design process. Physical models were also used (see photos, right).



Above: 3d model views used to explore options during design workshops

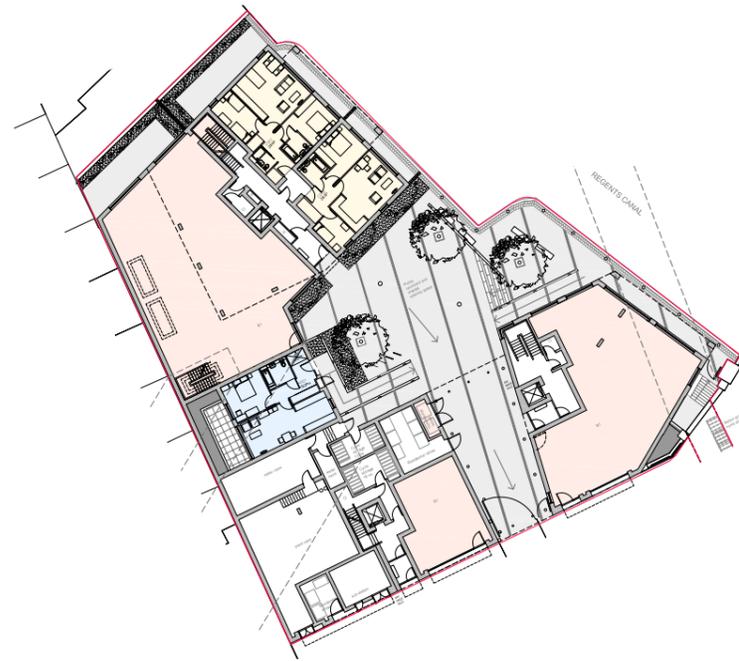


Above and below: photos of preliminary working model presented during design workshops

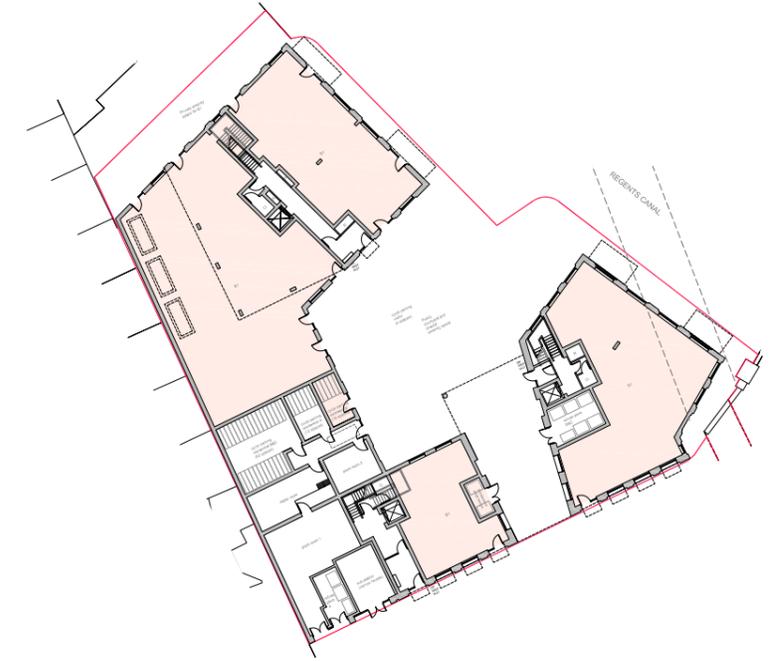


Following the previous planning refusal, and during the pre-application meetings and workshops, a number of significant changes were made to the scheme. These included:

- increase in the provision of employment space (from 686m<sup>2</sup> GEA to 801m<sup>2</sup>), mainly through removal of three ground floor apartments
- reduction in residential accommodation from 46 to 40 new dwellings
- reduction in massing of the western end of Georgiana Street to create a three-storey transition building
- change in massing between the two canalside buildings - the southernmost building reduced by one storey to five storeys (through removal of the setback top floor) and the northernmost building increased by one storey to six
- use of traditional warehouse typology in design of the principal elevations
- application of regular window arrangement within a grid rigorously applied around the scheme



Above: previous application's ground floor plan



Above: current application's ground floor plan



Above: previous application's canal frontage elevation



Above: current application's canal frontage elevation



Above: previous application's street frontage elevation



Above: current application's street frontage elevation

# Design & Access Statement

## 4.00 The proposals

### 4.01 Site strategy

At the outset of the project, a number of ways of approaching the redevelopment of this site were investigated..

The first involves arranging the majority of accommodation along the canal frontage and creates an 'L-shape' (Option One, right).

- |  |                                    |
|--|------------------------------------|
| Pros:  | Cons:                              |
| - waterside dwellings/workspaces                       | - single aspect dwellings          |
| - views up and down canal                              | - overshadows canal                |
| - creates large private courtyard                      | - no opportunity for public access |
| - maintains privacy to Royal College Street properties |                                    |
| - active frontage to street                            |                                    |

Option Two includes more accommodation at the northern end and pulls the building away from the canal edge in the centre, creating an 'H-shape':

- |                                  |   |
|----------------------------------|---|
| Pros:                            | Cons:                                   |
| - waterside dwellings/workspaces | - some single aspect dwellings          |
| - views up and down canal        | - some overshadowing of canal           |
| - opens up aspect onto canal     | - limited opportunity for public access |
| - active frontage to street      |   |

Option Three opens up the canalside and places all accommodation against northern and southern boundaries, forming a 'U-shape'

- |   |       |
|---|-------|
| Pros:   | Cons: |
| - all homes potentially have canal views                              |       |
| - most homes can be dual aspect                                       |       |
| - provides open aspect to the canal                                   |       |
| - creates large public open space                                     |       |
| - active frontages inside and out                                     |       |
| - allows views to/from the canal and Royal College Street properties. |       |

For the above reasons, the scheme was developed along the lines of Option Three.



Above an example of the approach taken in Option One with continuous wall of development - just south of Grays Inn Bridge



Above an example similar to the approach taken in Option Two with central development set back - south of Grays Inn Bridge



Above an example similar to the approach taken in Option Three with buildings on either side of an open courtyard - Lawfords Wharf (other side of Royal College Bridge)

## 4.02 Use

The proposal is to redevelop the site for residential-led mixed use: B1 offices at ground floor with B8 storage located within the existing tunnel and with 40 new dwellings at first floor and above.

**Employment use:** the proposals provide a total of 868m<sup>2</sup> of B1 office and B8 storage space GEA (see schedule, right). The new B1 workspace is designed to be flexible and can be let as whole units or sub-divided for smaller units as required (see Appendix 1 for details). The B8 space can be let to one user or fitted with individual lockable storage units and let to different B1 users for archiving and/or general storage.

**Residential use:** The residential accommodation is provided as a mix of 1, 2 and 3 bedroom apartments. These are designed to meet the Mayor's Housing SPG 2015: all 1 bed apartments are min 50m<sup>2</sup>, all 2 bed/4p apartments are min 70m<sup>2</sup>, all 3 bed/4p apartments are min 74m<sup>2</sup>, and all 3 bed/5p apartments are min 86m<sup>2</sup> (see detailed schedule of accommodation included in Appendix 2).

**Density:** The site has a PTAL of 6a and an area of 0.18ha. The area of residential is equivalent to 78% of the total proposed GIA of the development. Proposed density therefore is 285 d/ha, a little above the higher end of the range for Urban at this PTAL included in the latest London Plan.

**Affordable housing:** The proposal includes 18% of the new dwellings (by GIA) will be provided as affordable accommodation (see Viability Report for further details).

**Dwelling mix:** Camden Policy DP5 sets out a Dwelling Size Priorities Table. It states that the Council '...will seek to ensure that all residential development contributes to meeting the priorities set out in the Dwelling Size Priorities Table..' and '...will expect a mix of large and small homes in all residential developments'. However it goes on to say '...the Council acknowledges that it will not be appropriate for every development to meet the aims set out in the Priorities Table...' and 'Where possible a mix of large and small homes should be included within each category of housing....' and 'The Council will be flexible when assessing development against policy DP5, the dwelling size priority table and the aims set out in para 5.5'. The policy aims to include 50% large homes (ie 3 beds +) for Social Rent and 40% as 2 bed in Private Sale apartments. The proposals include 2 x 3 beds for Social Rent which is 100% and exceeds policy, or 4 x 3 beds for Affordable and Social Rent which equates to 66% provision. The proposals include 13 x 2 beds for Private Sale which is 38% of the total and therefore very close to the 40% aim. The previous application had a higher number of 3 beds for Private Sale but some of these were provided as penthouses setback on the top floor and have been lost during negotiations with officers over design (see below). The proposals therefore provide a good spread of unit sizes for each tenure and an appropriate mix of small and large new homes.

**Access & inclusive design:** 90% of the apartments are designed to meet Part M4(2) of the Building Regulations and 10% (4 apartments) are designed to meet Part M4(3) ie to be easily adaptable for full wheelchair use, should that be required. These are provided as Private Sale apartments, which means the provision exceeds the requirement for this tenure. The Affordable Rent/Social Rent building only contains 6 apartments, making the policy requirement provision of 0.6 dwellings for this tenure. The previous application included a fitted-out wheelchair apartment at ground floor but it was agreed with Officers

## Schedule of accommodation

| PROPOSED EMPLOYMENT SPACE                |         | sqm        |     |
|--|---------|------------|-----|
| B1                                       | OFFICE  | 813        |     |
| B8                                       | STORAGE | 55         |     |
| <b>TOTAL AREA GEA (EMPLOYMENT SPACE)</b> |         | <b>868</b> | sqm |

| PROPOSED RESIDENTIAL |                |  |       |  |                       |
|----------------------|----------------|--|-------|--|-----------------------|
| UNIT TYPE            | TOTAL PROPOSED |  | % MIX |  | AFF RENT<br>No. Units |
|                      | No. Units      |  |       |  |                       |
| 1B [2P]              | 16             |  | 40%   |  | 0                     |
| 2B [4P]              | 15             |  | 38%   |  | 2                     |
| 3B [4P]              | 2              |  | 5%    |  | 2                     |
| 3B [5P]              | 7              |  | 18%   |  | 0                     |
| <b>TOTAL UNITS</b>   | <b>40</b>      |  |       |  | <b>4</b>              |

| TOTAL AREA GIA  |  | sqm      |     | AFF RENT  | SOC RENT  | INTERMED  | PRIVATE   |
|---|--|----------|-----|-----------|-----------|-----------|-----------|
|   |  |          |     | No. Units | No. Units | No. Units | No. Units |
| TOTAL AREA GIA  |  | 2773     | sqm | 314       | 178       | 0         | 2281      |
| TENURE SPLIT (AREA, GIA)                                  |  |          |     | 11%       | 6%        | 0%        | 82%       |
| TOTAL AFFORDABLE SPLIT (AREA, GIA)                        |  | sqm 492  |     | 100%      | 0%        |           |           |
| TOTAL AFFORDABLE GIA (soc rent + aff rent + intermediate) |  | 492 sq m |     |           |           | 18%       |           |
| TOTAL WHEELCHAIR ADAPTABLE (Part M4(3))                   |  |          |     |           |           | 4         | 10%       |



that this was not a suitable location for such a dwelling and One Housing Group are unable to provide a fully wheelchair-accessible apartment above ground floor without two lifts, which would not be feasible for a total of 6 dwellings. (See Access Statement below for further details, including a plan of the easily adapted apartment for full wheelchair use).

The scheme is designed to be car-free, given its high 6a PTAL rating. There are 64 secure, internal bicycle storage spaces (as per latest London Plan requirements) for residential use and 10 spaces for B1, plus 4 visitor spaces in the courtyard. (See Access Statement below for further details).

### 4.03 Layout

Proposed layout and orientation of the buildings were generally supported during the previous application, subject to detailed design.

The proposals follow Site Strategy Option Three ie U-shaped buildings arranged around a courtyard (see page 26).

The approach to the layout begins with the central landscaped courtyard opening onto the water (Figure 1).

The existing buildings at Eagle Wharf have differing relationships to the canal edge: set at angles to one another and rising directly out of the water or set back from the edge (photos below and Figure 2).

Using the above as a starting point, the proposed buildings are placed at slightly differing angles (following the changes in direction of the canal wall and acknowledging the inlet) and different distances from the canal edge. The buildings surround the central courtyard whilst leaving the canal edge open to allow maximum daylight and sunlight to reach the water (Figures 3 and 4).

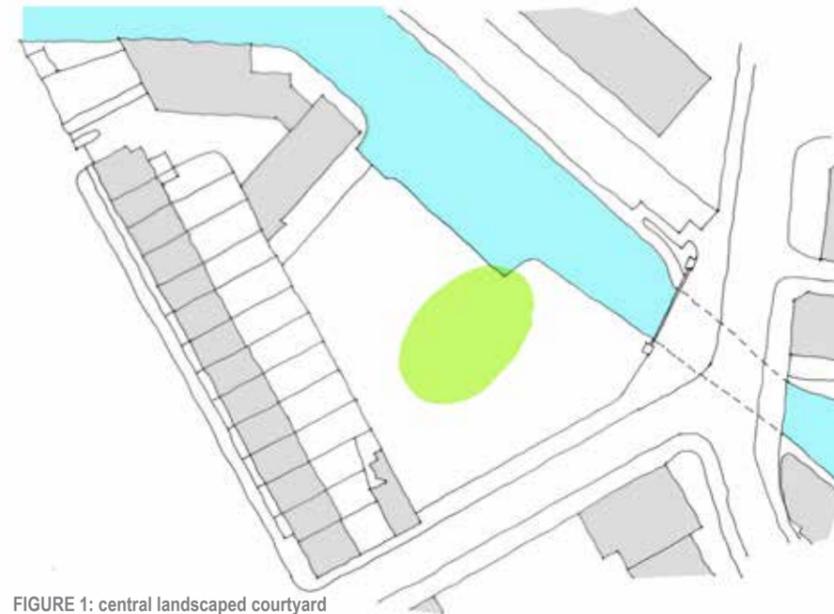


FIGURE 1: central landscaped courtyard



FIGURE 2: existing edge conditions



Existing edge conditions at adjacent Eagle Wharf

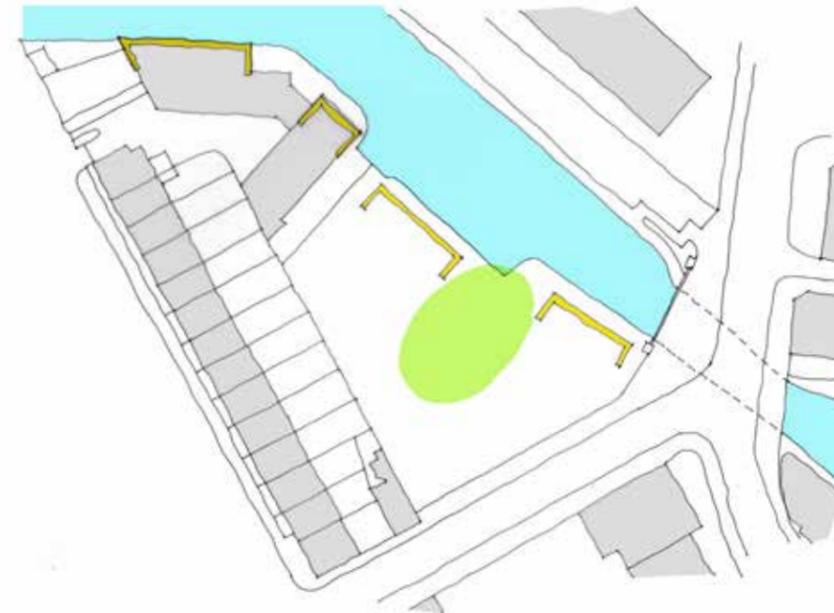


FIGURE 3: proposed edge conditions

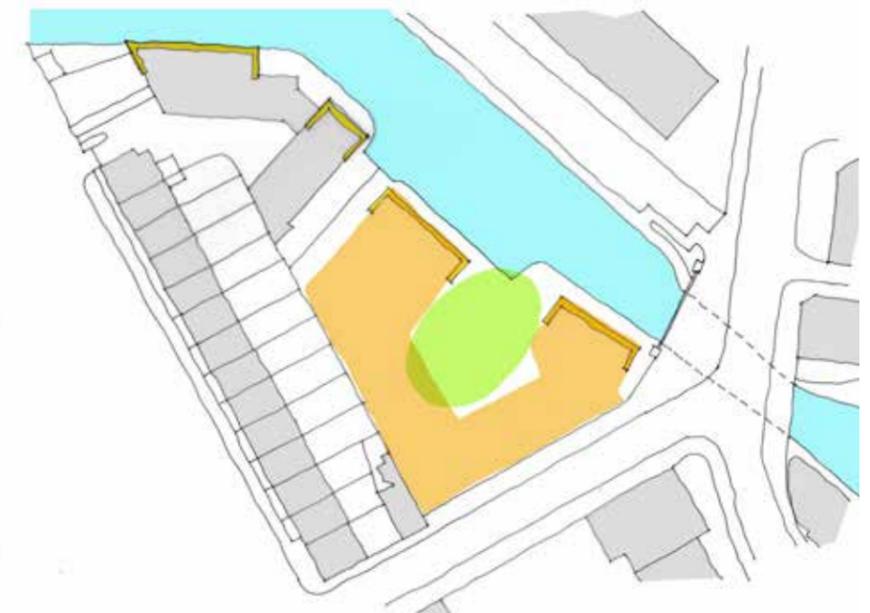


FIGURE 4: proposed buildings

The building footprint allows views in and out of the development, to and from the tow-path on the opposite bank. The proposals include a generous, ground floor entry point from Georgiana Street which also allows views to connect the street to the courtyard and the canal beyond (Figure 5).

The massing of the proposed buildings is such that upper floors are placed along the street frontage and canalside, leaving a single storey only against the Royal College Street property boundary, maintaining good levels of daylight to their windows (Figure 6).

Upper floors maintain a minimum distance of 18m between windows of habitable rooms facing one another from the proposed buildings and the existing in Royal College Street houses, ensuring no overlooking or lack of privacy to existing neighbours (Figure 7).

The layout of the proposed buildings maintains views of the buildings that make a positive contribution to the Conservation Area from the public realm. These are The Constitution PH still visible along Georgiana Street and the buildings at Eagle Wharf still visible from Grays Inn Bridge (Figure 8).



FIGURE 5: proposed building footprint and views in and out



FIGURE 6: building massing allowing views and daylight to Royal College Street properties



Existing views of 'buildings that make a positive contribution' in the CA: Eagle Wharf as seen from Grays Inn Bridge (left) and The Constitution PH as seen along Georgiana Street (right)

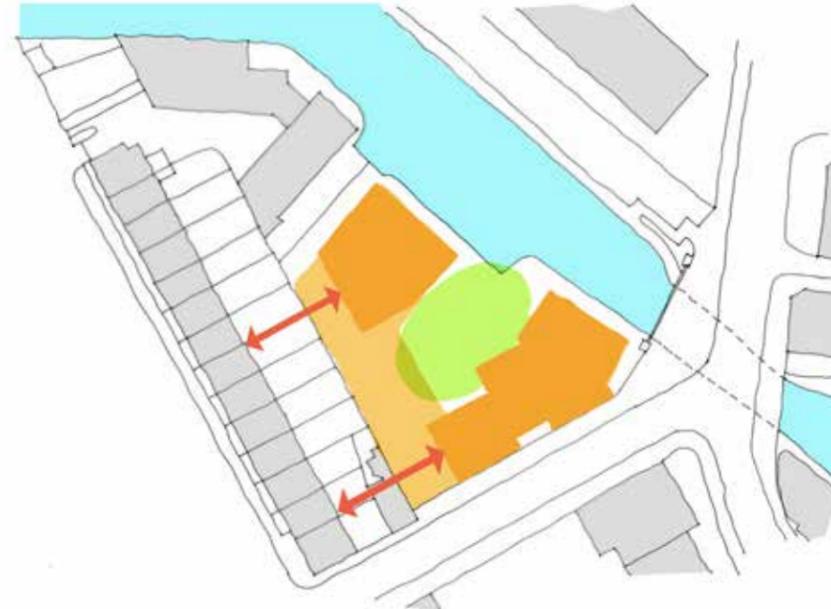


FIGURE 7: overlooking distances minimum 18m between windows to habitable rooms

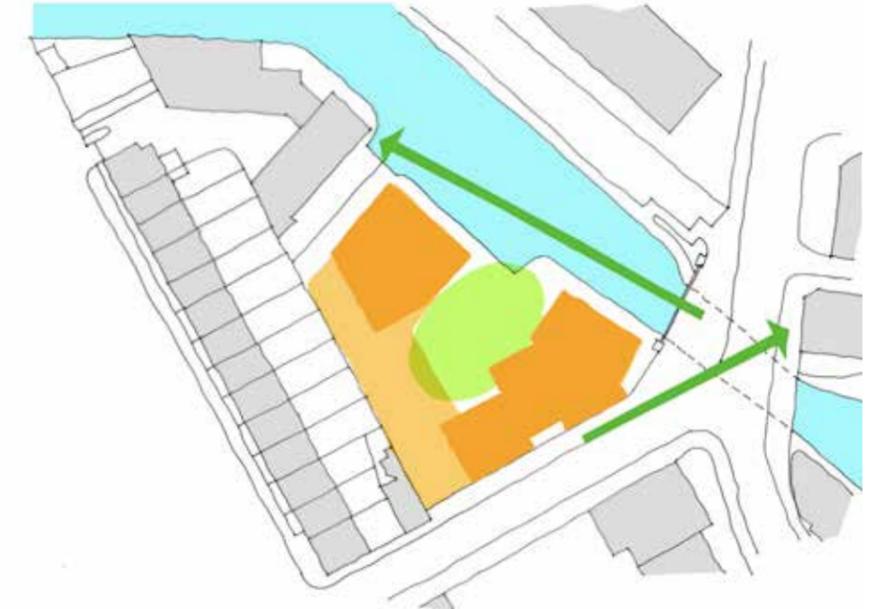


FIGURE 8: maintaining views of buildings that make a positive contribution in the Regents Canal CA



Shadow studies of the proposal: left 3pm 21 March; right: 3pm 21 June

### 4.03 Layout (continued)

The proposal sits the new building at back of pavement on Georgiana Street to provide enclosure to the street. The new open (public) access through to the courtyard and entrances (commercial and residential) along the street give an active frontage.



The entrance way has windows and a residential entrance and there are further commercial entrances with another residential entrance opening directly off the courtyard. These combine to provide extensive **active frontages** to the majority of the ground floor.



The servicing elements of the scheme have been located in the most discrete locations. Those elements that have to be sited on the street frontage (sub-station, plant and refuse for the adjacent residential entry) are contained within the short frontage of the three storey building. This means that the majority of the frontage is fully active (Figure 9). Other servicing (refuse) is within the entrance way or accessed from the courtyard (further plant and cycle storage).

The majority of the apartments are **dual aspect** - the only ones that are not dual aspect are 4 x 1 beds that face south-east ie are not north-facing, not exposed to high noise levels and are not family dwellings (Figure 10). The proposed layout, with apartments located in two separate buildings, also means that all proposed habitable rooms will meet BRE Guidelines for **daylight** and every apartment in the scheme has good **aspect** with a view of the canal, including the affordable homes (Figure 11).

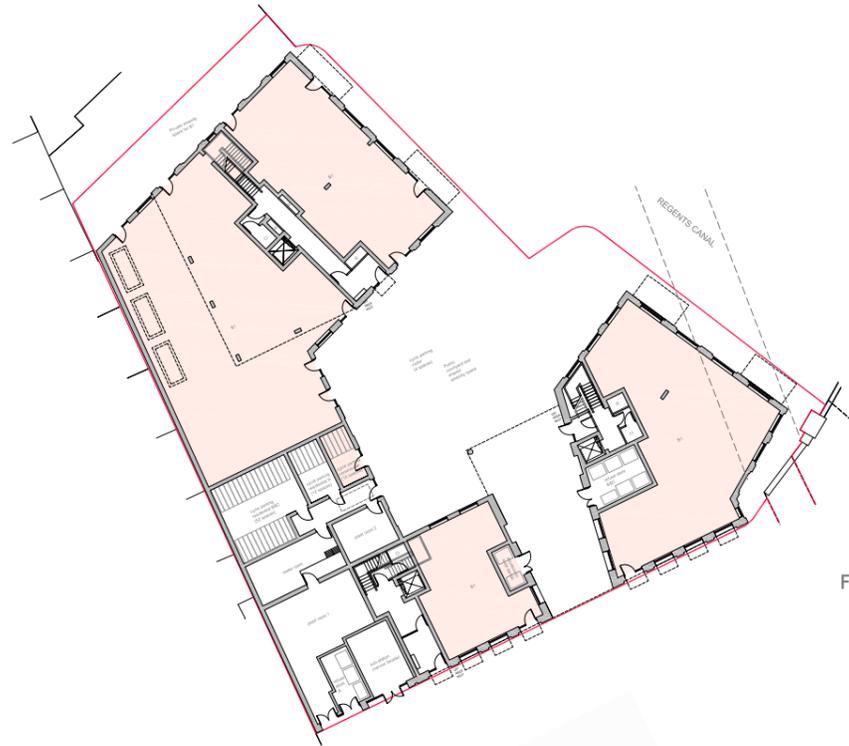


FIGURE 9: ground floor plan and active frontages diagram



FIGURE 10: upper floor plan showing dual aspect for all but one apartment



FIGURE 11: upper floor plan showing canal views from all apartments

Existing **grain** is reinforced through the layout of the new buildings: see existing and proposed grain in the figure ground diagrams (Figure 12). The layout opens up the wharf and new buildings face the water, reconnecting the wharf with the canal and improving views of the site, both from the towpath and the waterway itself.

The courtyard will provide public access to this side of the canal for the first time. It will also allow good levels of sunlight and daylight to the water (see Daylight & Sunlight Report for details). It will maintain views of the majority of the roovescape of the rear of Royal College Street properties when viewed from the towpath, as well as views of the canal from most of these properties.

The diagram (below, far right) shows a comparison of the new opening between buildings on the waterside between the proposal and that found at Lawfords Wharf a little way along the canal. The openings will be very similar in width (Figure 13).

**Amenity** for existing residential neighbours is maintained, with good levels of daylight (see Daylight & Sunlight Report for details) and privacy for existing occupants. The proposed residential accommodation will also achieve good levels of daylight and the majority of new dwellings meet minimum overlooking distances. CPG 6 says that there should normally be a minimum distance of 18m between the windows of habitable rooms of different units that directly face each other. Where windows do directly face one another (Building A and Building C), the minimum 18m is achieved between windows, although balconies are closer. Buildings B and C do not directly face one another as the buildings are angled and minimum distances are not achieved (although they are close in some cases). Discussion regarding the inclusion of blank windows, to help alleviate this, was held with Design Officers who agreed the building design was improved by including full windows, rather than blanks, so these have been retained.

Officers have asked for the removal of a window in Building B to improve privacy between it and the balcony to the rear of Building A, but this would make the former single aspect, north-facing so it has been left as proposed.

Given the agreed approach to detailed design of the parts of Buildings B and C that face one another (continuation of grid and openings) and the constrained nature of this urban site, it is argued that, although not fully compliant, the benefits outweigh the need to fully comply with CPG 6 in this case.



View of model showing proposed opening onto canal



Existing opening onto canal at Lawfords Wharf



FIGURE 12: Figure ground diagrams: existing (left) and proposed (right)

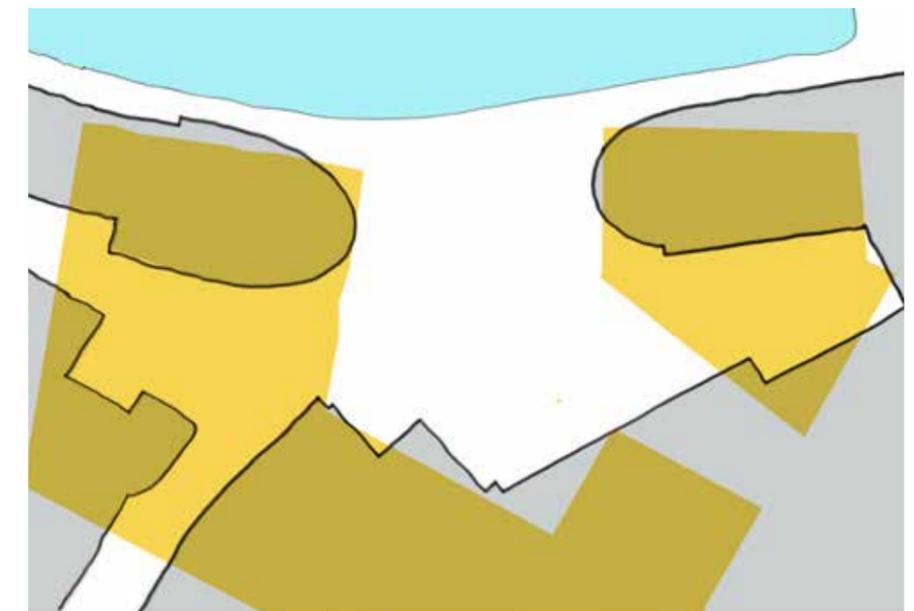


FIGURE 13: Diagram showing plans of proposed Bangor Wharf and existing Lawfords Wharf superimposed for comparison: Bangor Wharf (yellow), Lawfords Wharf (grey)

#### 4.04 Scale and massing

The scale of proposed buildings was generally supported during the previous application, subject to detailed design. The buildings were a mix of 1-6 storeys.

During the extensive pre-application period for the second application, discussions took place regarding building height following receipt of comments from the Design Review Panel in August 2016. After review however, Officers confirmed their support for the proposed scale of 1-6 storeys but asked for a series of options for the treatment of the proposed fifth floor on Building B. This was proposed as a setback top floor for which a number of treatments were put forward. However, after further options and review, it was agreed to omit the top floor on this building altogether and to add a part-fifth floor on to Building C instead. Building B is more prominent in views of the scheme from several directions and having it as a five-storey building reduces the impact of the scale and massing on these views.

The impact of adding a fifth floor to Building C is reduced, partly because it is further away from the bridge, is therefore less prominent in the views, and partly because it is set further back on the site due to the inset section of the canal edge. This means that it is also set back behind Eagle Wharf which remains dominant when viewed along the canal towpath or from the bridge.

The massing creates two distinct buildings which front the canal, linked at ground level to the rear of the site with a large open space between, allowing views in and out of the development.



Aerial model view of the proposal from the east



Model view of the proposal from the west



Computer model view from the west



Computer model view from Grays Inn Bridge looking west



Eagle Wharf

The massing of the Georgiana Street frontage also underwent a number of changes during the pre-application design workshops.

As explained earlier, the massing of the western end of this frontage was revised and reduced to read as a distinct, three storey building, acting as a transition between the lower scale of the existing buildings and the proposed. It also ensures that good levels of daylighting are provided to the rear of Royal College Street properties.

The massing of the taller, five-storey building now reads as two 'buildings', separated by the deeply recessed area above the entrance way. Following a change in the layout of Building B, giving it a perpendicular rather than angled relationship at the eastern end on Georgiana Street (see page 36), these two 'buildings' are now the same width and height, producing a harmonious appearance when viewed along Georgiana Street. The deep recess is spanned by full-width balconies, also recessed from the front of the building. This results in balcony depth that is 1.2m (less than 1.5m required by the Mayor's Housing SPG), but this has been discussed and agreed with officers as acceptable in this case in order to produce the required articulation. The balconies are 1.5m<sup>2</sup> bigger than the required minimum however (see Amenity section below for details) due to their additional length, mitigating the impact of the shallower depth. The parapets to the 'buildings' return beyond the extent of the recess to ensure the buildings read as separate units.

In addition to the changes noted above, the overall height of Building B was reduced during discussions, following agreement that there would be no fire engine access to the courtyard - this reduced the building by approximately 0.75m or 10 brick courses. Following removal of the penthouse level on Building B, it was also agreed to reduce the height of parapets to all buildings, reducing the height throughout by a further 5 courses.

In considering proposed scale and massing for this site, there are two buildings in the immediate vicinity which make a positive contribution, according to the Conservation Area Appraisal document, which are The Constitution PH and Eagle Wharf (see photographs below and opposite page). The proposals have considered the impact on these.

In the case of the PH, it is located at the end of Georgiana Street, on the other side of Grays Inn Bridge, and the proposal will not interrupt views of this building. As a building which makes a positive contribution, however, the proposal should not impact negatively. The view (right) shows how the reduced scale and massing on Georgiana Street relates more directly to the scale of The Constitution.

Eagle Wharf has a closer relationship to the site: the existing and proposed buildings will be visible together from the towpath. As explained on the previous page, the siting of the proposed buildings, although taller, will not dominate the former warehouse which will still be prominent in all the principal views.

Having made the above changes to the scale and massing of the scheme since the previous application, the proposals strike an appropriate balance between preserving and enhancing the character of the Conservation Area and the need to maximise the opportunity for new housing.



Model view along Georgiana Street from the south-west



Model view along Georgiana Street from the bridge



Computer model view along Georgiana Street from the south-west



Computer model view up St Pancras Way looking north



The Constitution PH

## 4.05 Appearance

A series of pre-application meetings, design workshops and two DRP's since the previous refusal have resulted in significant changes to the approach to the design of the appearance of the proposed buildings.

The new approach began with a series of studies of traditional canalside and commercial warehouse architecture, using examples to be found in the surrounding area and along the Regents Canal. The photographs on this page and opposite illustrate some of these.

It was found they all displayed some or all of the following typical characteristics:

- clear expression of the buildings' function
- simplicity of materiality and form
- use of repeated opening types
- predominant use of London stock brick
- expression of traditional, loadbearing brick structure with arched window heads and sometimes with decorative string courses
- windows are regularly-spaced openings in brickwork with either flat or semi-circular arches; generally smaller openings above ground floor
- solid: void proportions express sense of solidity
- window openings on upper floors generally line up with ground floor openings
- metal windows, often multi-paned and symmetrical
- some use of full height 'loophole frames', or loading bays
- openings set into deep brick reveals - minimum half a brick
- eaves are level with pitched roof behind or have parapets and/ or are expressed as gable ends
- strong parapet line often with corncicing



Eagle Wharf as seen from the entrance of Bangor Wharf



Central section of Granary Building, Granary Square, Kings Cross



Above: Interchange Warehouse, Camden Lock

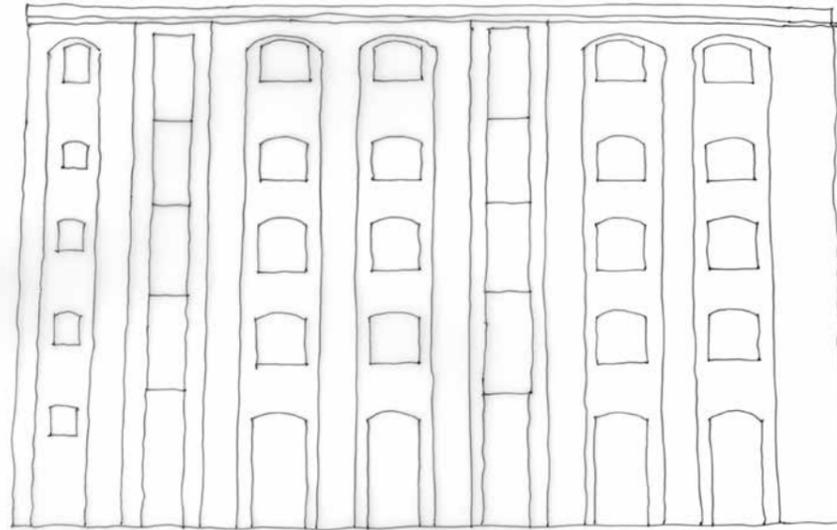
Below: Regents Wharf, All Saints Street, Kings Cross



The sketches below show the principal characteristics of the two types of warehouse typology: left is a canalside warehouse (Regents Wharf, All Saints Street) and right is a commercial warehouse a (Lymehouse Studios, Georgiana Street)

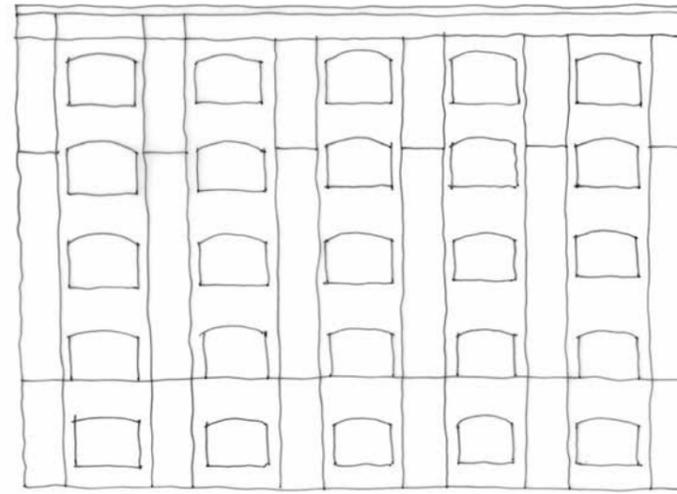
**SCHEMATIC OF TRADITIONAL CANALSIDE WAREHOUSE TYPOLOGY**

- LOADBEARING BRICK CLEARLY EXPRESSED
- LOOPHOLE FRAMES RUN FULL HEIGHT
- WINDOW OPENINGS AT REGULAR INTERVALS REPEAT AT UPPER FLOORS WITH TALLER GROUND FLOOR OPENINGS
- WINDOW OPENINGS LINE THROUGH ALL FLOORS
- WINDOW OPENINGS HAVE FLAT OR SEMI-CIRCULAR ARCHED HEADS
- CONTINUOUS PARAPET LINE



**SCHEMATIC OF TRADITIONAL COMMERCIAL WAREHOUSE TYPOLOGY**

- LOADBEARING BRICK CLEARLY EXPRESSED
- REPEATED ELEMENTS
- WINDOW OPENINGS AT REGULAR INTERVALS REPEAT AT UPPER FLOORS WITH TALLER GROUND FLOOR OPENINGS
- WINDOW OPENINGS LINE THROUGH ALL FLOORS
- WINDOW OPENINGS HAVE PERPENDICULAR LINTELS
- CONTINUOUS PARAPET LINE



Below: commercial warehouse buildings at Lymehouse Studios, Georgiana Street (left and centre) and Camden Garden Centre, St Pancras Way (formerly part of Grain Warehouse), right



#### 4.05 Appearance (continued)

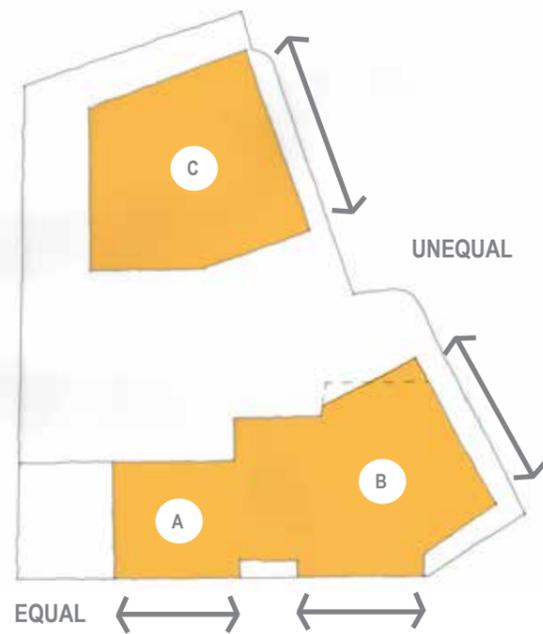
Analysis of the warehouse typology above resulted in a series of studies for the elevational treatment of the proposed buildings, incorporating many of the characteristics identified, thus drawing on the site's history and industrial heritage. A 'structural' grid has been applied to all faces of the principle buildings, with cantilevered balconies on all but the street frontage. This has the effect of simplifying and rationalising the different parts of the buildings, drawing them together and producing a more rigorous and coherent appearance.

The layout of Building B was amended in order to achieve the above by:

a) changing the Georgiana Street frontage so that either side of the recessed section (above the entrance) are equal and the eastern end of the building meets the street at right angles.

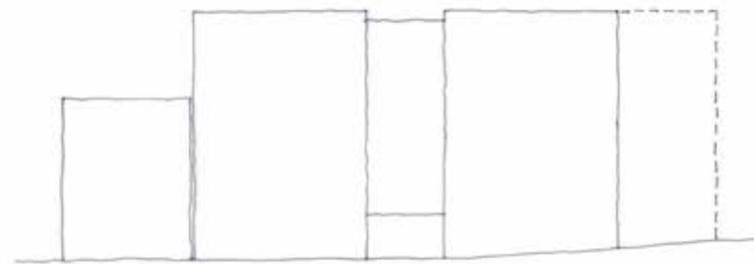
b) changing the canal frontage of Building B so that it has a perpendicular relationship at the northern end, rather than meeting it at an angle. This provides a more robust appearance to the building when viewed from the towpath. Building B and Building C's frontages were changed to be of unequal length after discussion with officers.

The diagram below illustrates these points.

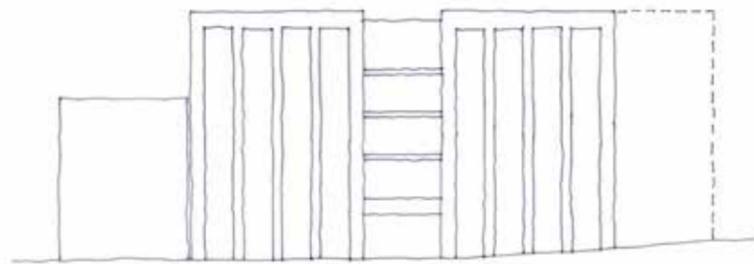


Above: diagram to illustrate evolution of layout (previous relationship of north side of Building B shown dotted)

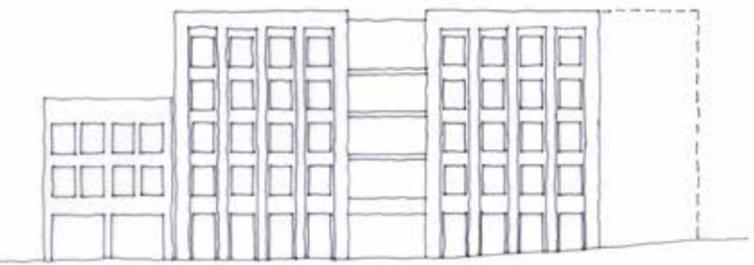
#### Georgiana Street frontage



Above: revised building massing

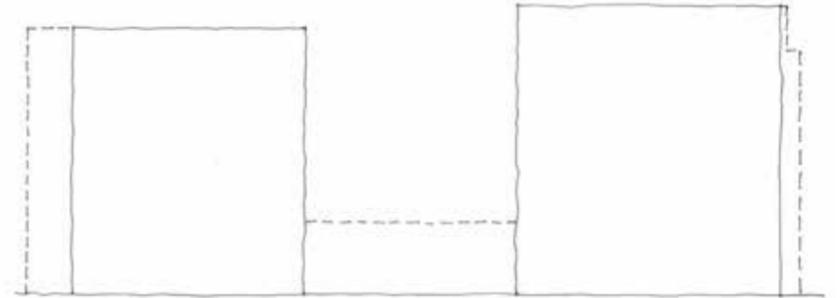


Above: 'structural' grid applied to building massing

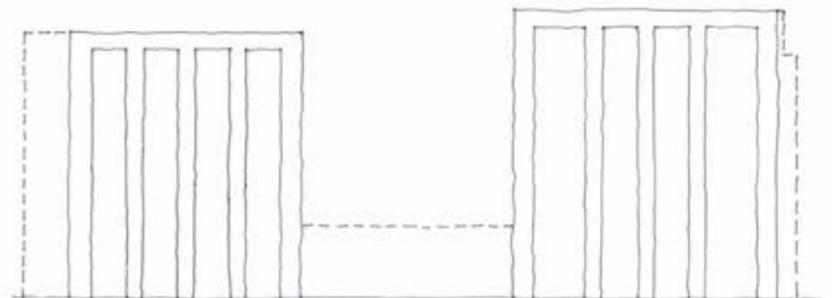


Above: window openings applied to 'structural' grid

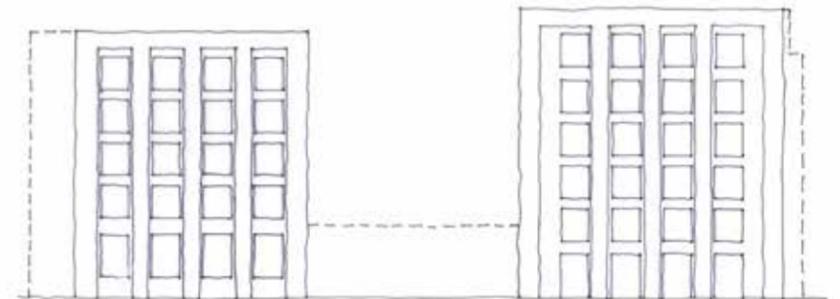
#### Canal frontage



Above: revised building massing



Above: 'structural' grid applied to building massing



Above: window openings applied to 'structural' grid



Above: Georgiana Street elevation as proposed

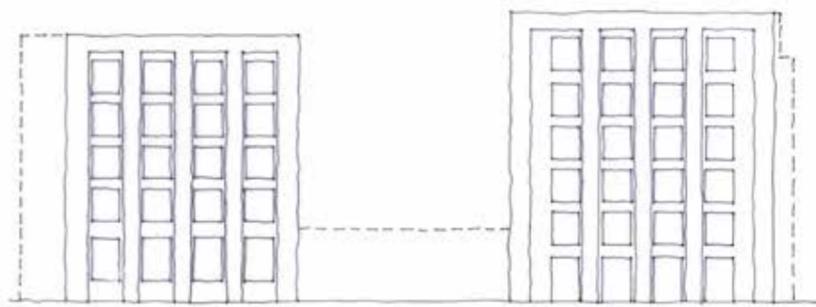


Above: canal elevation as proposed

## 4.05 Appearance (continued)

A few options for the number of recesses in the grid were put forward and it was agreed with officers that street and canal frontages should read as distinct from one another, whilst utilising the same language - there are four recessed panels within the grid on each building section on both frontages but those on the canal frontage are wider, allowing inclusion of larger windows (wider and full height). This gives added emphasis to this frontage, which is the principal public face of the scheme. The grid is repeated on all faces of the buildings to ensure consistency as they will be read together, when viewed from the towpath opposite. Studies of solid: void were tested for each option - ensuring a robust, industrial appearance (see earlier and final versions, right).

In addition, there are differences between the grids applied to the two buildings facing the canal frontage. Through discussion with officers, it was agreed that the two elevations should be unequal widths (Building C, the taller of the two, is wider) but, as the window openings are the same in each, the outside recesses on Building C are wider to take up the difference. The window openings are placed asymmetrically in this part of the grid to maintain the regular rhythm across the buildings. Also the ground floors differ between the two buildings (due to the structural limitations of building over the Fleet sewer) providing further, subtle visual contrast between the two.



Above: subtle differences in composition between the two canal frontages

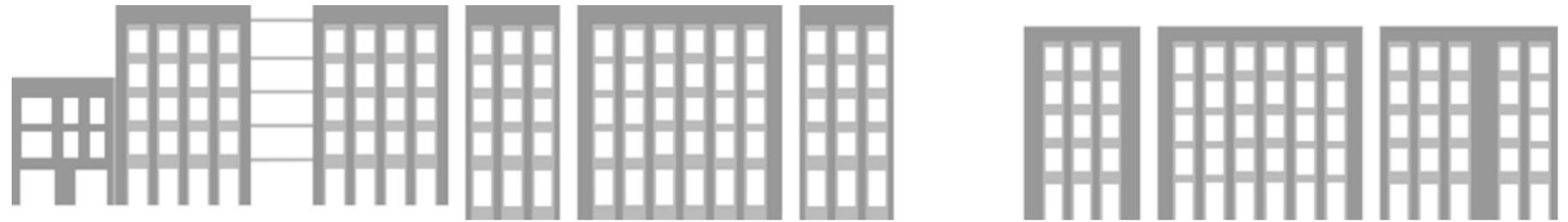
The proposals include three types of balcony: cantilevered, cantilevered with suspension rods and inset. The latter are used on the Georgiana Street frontage in the recess above the site entrance and in the three storey building at the western end. Elsewhere all balconies are the cantilevered type but with suspension rods to those on the canal frontage, partly because they are required to support the deeper balconies on Building B, but also to give these balconies a more prominent appearance.

A number of options for the treatment of balustrading were reviewed with officers and it was agreed to include plain, vertical steel flats to maintain the industrial aesthetic.

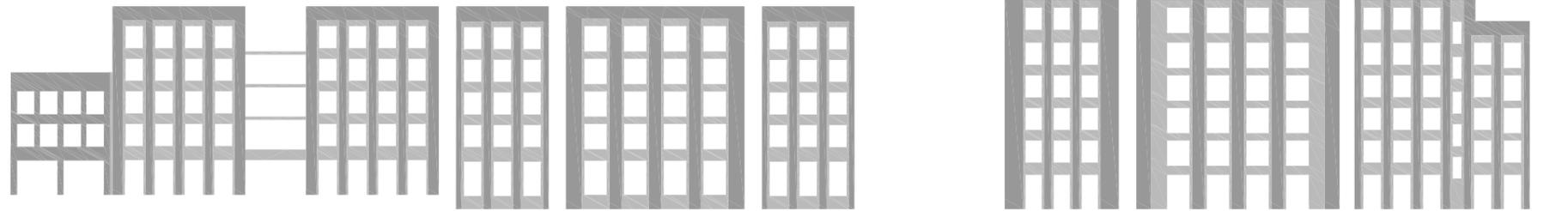
Juliet balconies are included to raised ground floor windows on Building B with decorative brick spandrel panels beneath to create visual interest.



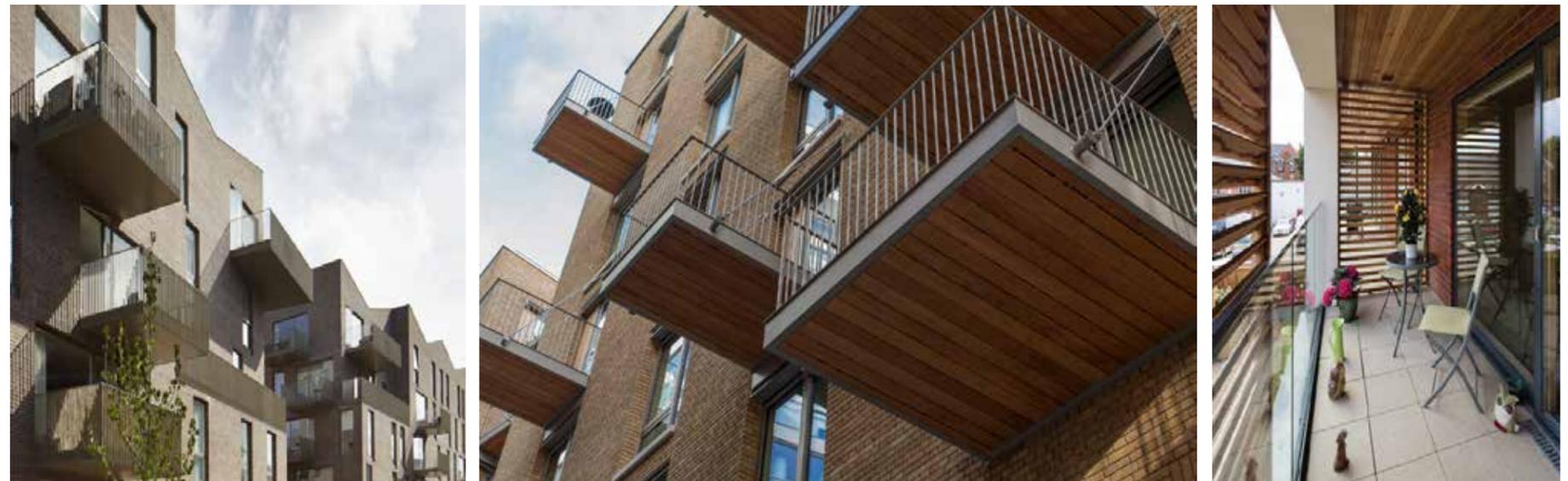
Left: detail of blue engineering brick with projecting headers



Earlier version with option for greater subdivision of 'structural' grid and earlier massing



Final, agreed solid: void study, showing the difference in solid: void on the canalside, giving greater emphasis to these principal elevations



Three balcony types proposed l-r: cantilevered (Duggan Morris), cantilevered with support rods (Squire & Partners), inset (AHMM)

#### 4.05 Appearance (continued)

The three storey building at the western end of the site, which acts as a transition in scale between the existing and the main part of the proposals, is treated differently from the rest. It is the same scale and takes its references for window openings from the surrounding residential terraced typology, rather than the canalside warehouse one. Within the three storey 'envelope' are contained some of the servicing elements that need to be placed on the street frontage - they are concealed behind louvred doors, deep-set into recesses in the ground floor. These will be masked in long views by the projecting ground floor of no 54 Georgiana Street. Above are pairs of windows or inset balconies, the latter with white-painted brickwork within the recess. The proposal is for plain, blue-black engineering brickwork to distinguish it from the adjacent, larger scale development but using one of the same two facing materials. Brickwork detailing on this part of the scheme is simple and contemporary. Balcony balustrading will be vertical steel flats, to match those elsewhere.

The rest of the street frontage comprises brick elevations with the regular grid described above. These will be built in yellow London stock brickwork with blue engineering brick plinths to 'ground' the buildings on the street. The window openings are set within the grid, as repeating elements, articulated by the recessed balconies above the main entrance. The ground floor comprises a series of fully-glazed shopfronts, deep-set into recesses which are defined by aluminium reveal liners with brise soleil, and the entrance gates to the courtyard. The reveal liners emphasise the shopfronts, enhancing their presence and appearance on the street frontage. One of these shopfronts is a residential entrance, which will be differentiated from the commercial units through the use of an extended brise soleil that doubles as a canopy to protect residents from the weather.

The building frontage to Georgiana Street is divided in two by a deep, full-height recess into which the brickwork faces return, reinforcing the separation. Between are balconies, recessed from the face of the building to accentuate this articulation. Behind these is full-width glazing to apartments fitted with sliding, louvred screens to control solar gain. Between and above the glazing are aluminium panels finished with a dark grey, matt powder coating, the same as used on the fascias to the balconies. Balcony balustrading comprises vertical steel flats. The treatment of this recess is in marked contrast to the solidity of the flanking buildings and makes reference to the industrial heritage of the site.



Above: detailed drawing of the three-storey building



Above: model view along Georgiana Street, showing how service doors to the ground floor of the 3-storey building are masked by projecting ground floor of no 54 Georgiana Street.

At the site entrance below are gates, which will be open during daylight hours, allowing public access to the courtyard and canal. The gates are made up of vertical steel flats, similar to those used in the balcony balustrading, but twisted through 90° at the base so that they change from perpendicular to parallel they rise, increasing visual interest.

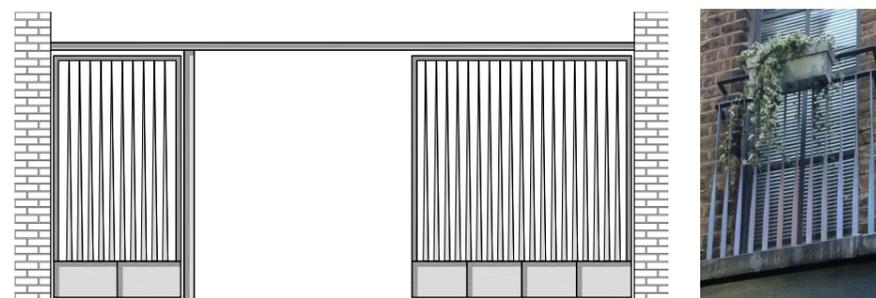
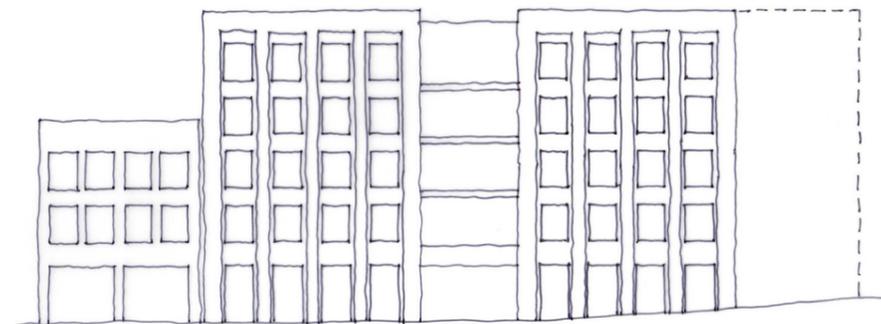
Building signage will be displayed above the gates with stainless steel lettering on spacers making the letters stand proud from the panelling behind.



Above: view of the main entrance on Georgiana Street

The entrance way itself is a wide and generous opening with recesses to the flank walls, designed to give visual continuity with the openings along the street frontage. This is reinforced by the use of aluminium baffles on the soffit of the first floor slab, which line through with the recesses and contain concealed uplighters. Brickwork panels between the recesses will be highlighted with uplighting wall-washers buried in the paving.

The above will combine to create a welcoming entrance for residents, employees and visitors alike.



Above: detail of gates for Bangor Wharf; right: twisted steel flat balustrade to balcony in Shoreditch



Above: entrance gates at Saxon Court (Maccreanor Lavington)



Above: view into courtyard from Reachview Close

Discussions were held with officers regarding potential treatments for parapets around the buildings. After reviewing a number of options, it was agreed to use the combination of vertical banding of matching facing brickwork finished with a colour coated aluminium coping section to give a strong, continuous definition to parapets. This will apply to all buildings, except the three storey one which will simply have the coping.

Below: agreed parapet treatment and built example of vertical brick banding and aluminium coping to parapets (Hawkins Brown)



Above: part elevation showing brick detail to parapet (left); similar example by Hawkins Brown (right)

Brickwork detailing is proposed as a contemporary interpretation of traditional brick detailing. As explained above, the elevations of all but the three-storey building refer to warehouse typologies through the use of regular recessed bays to express the 'structural' grid. These bays will be constructed with half brick reveals and windows will be set back by another half brick, adding depth and visual interest to the elevations.

Facing brick will generally be stretcher bond, but with vertical brickwork above windows (2 bricks high), making reference to traditional arches. At the head of each bay, this detail will be extended to 3 bricks. The latter will also be used to finish parapets (see below left).

Photos of the types of bricks proposed are included on page 41. Pointing will be bucket handle for the stock brickwork and flush for the engineering brick. Mortar colour is proposed as natural.



Part Elevation - Block B - Canalside  
Scale 1:25



Part Plan - Block B Typical Floor - Canalside  
Scale 1:25

Above: excerpt from one of the detailed drawings

Below: view of the proposal from Grays Inn Bridge



#### 4.05 Appearance (continued)

The CGI photomontage (right) shows the impact of the proposals on one of the key views along the canal. (We have not shown a CGI of the other view included in the previous application as, since the building has been reduced in height, lost its penthouse floor and its framing on the canal frontage, it is no longer visible in this view - the original photo is shown, bottom left).

The image shows the buildings facing the canal when viewed from the towpath as it curves around, just after Royal College Street Bridge. The previous application's CGI is shown below for comparison (although from a slightly different viewpoint). The reduction in Building B's overall height and the loss of its penthouse level means that layout of the buildings, with the generous courtyard between, ensures that the open nature of this part of the Conservation Area is more clearly seen now, even though Building C is taller.

The Constitution PH (a building that 'makes a positive contribution') is just visible over the trees on the left hand side of the image and this illustrates how views of it are unaffected by the proposals on the opposite bank.

The new willow tree planted on the water's edge, together with the proposed planting in the courtyard, will mitigate the loss of the existing (damaged) weeping willow, softening the canal edge and providing opportunities for biodiversity, as well as being part of the site's SuDS strategy.

Bottom right is a CGI view of the proposals from the other direction, at street level, looking over the bridge on St Pancras Way. This view benefits from the reduction in height, following the removal of the top (penthouse) floor.



Above: CGI photomontage of the proposals as seen from the canal towpath, looking south-east



Above: viewpoint for previous CGI - proposed building no longer visible due to changes to height and massing



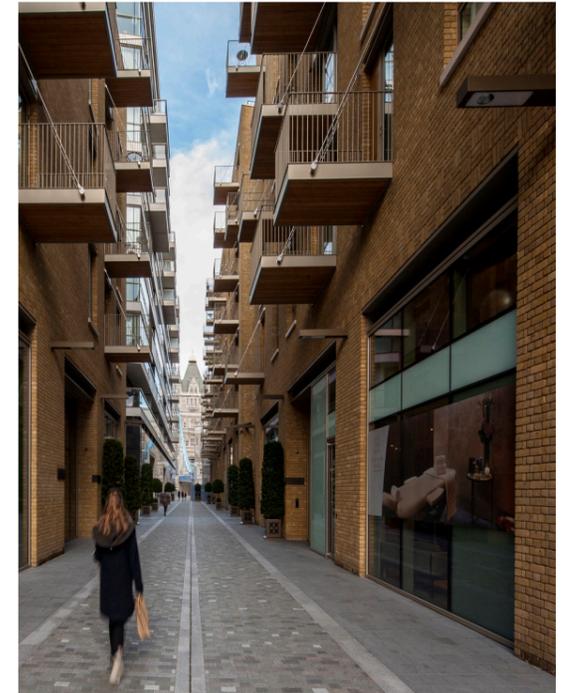
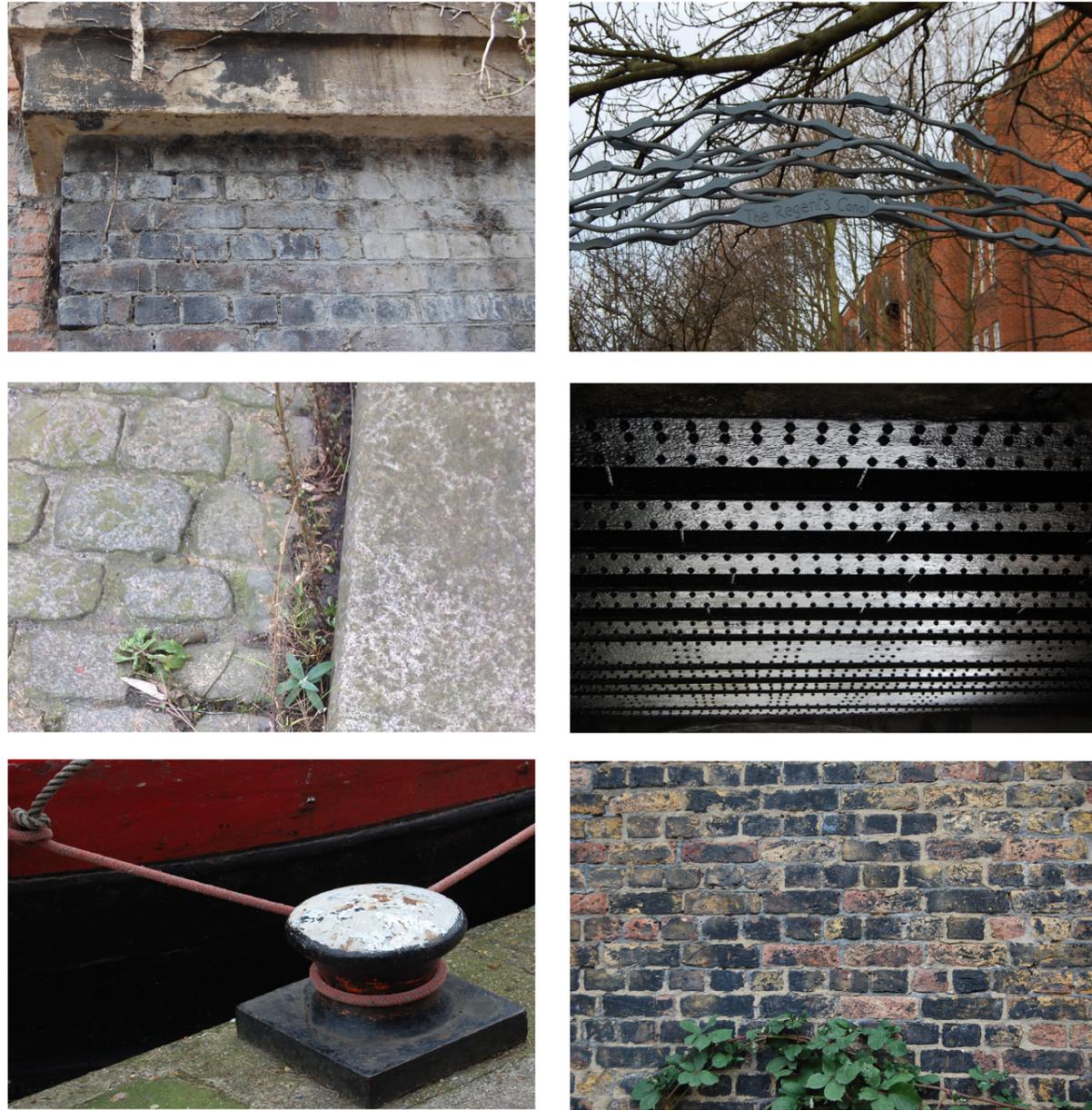
Above: CGI of previous application proposal from (almost) the same viewpoint as above



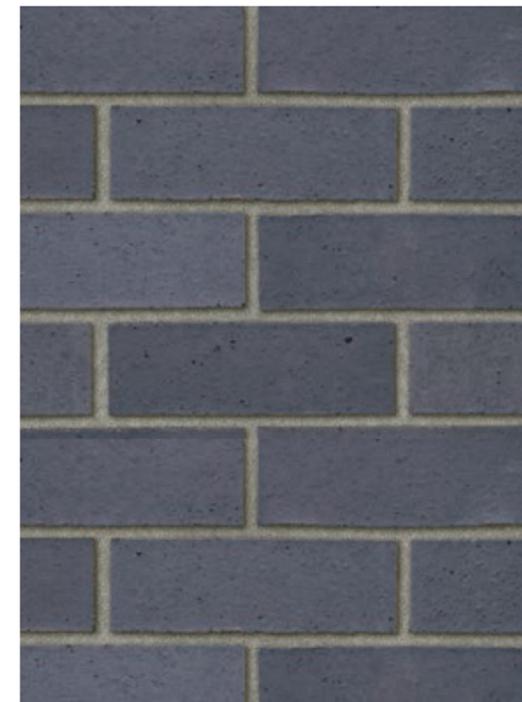
Above: CGI of proposal looking over the bridge from St Pancras Way

#### 4.05 Appearance (continued): materials

Below is a collection of photographs showing materials, textures and artefacts found in the immediate vicinity of the site. These have been used to inform the proposed palette of materials shown right: stock brick and blue/black engineering brick to reflect the industrial heritage of the site.



Above and right: Smead Dean type London stock brick;  
Below: blue/black engineering bricks and the same brick with projecting headers (right)



The main facing brick is a traditional London stock that will complement the variety of brickwork to be found in the vicinity. This will be combined with a blue/black engineering brick used as a plinth for the buildings or, in the case of the three storey building at the western end of Georgiana Street, for the whole building. Spandrel panels beneath the ground floor windows on the canal side of Building B will have projecting headers to provide textural interest.

The recessed section above the entrance on Georgiana Street, which has full width windows at the rear of balconies, will comprise a mix of matt-finished PPC aluminium panelling and steel, making reference to the site's industrial heritage.

## 4.06 Amenity

Camden planning policy requires two types of amenity space: under CPG2 Housing and CPG6 Amenity.

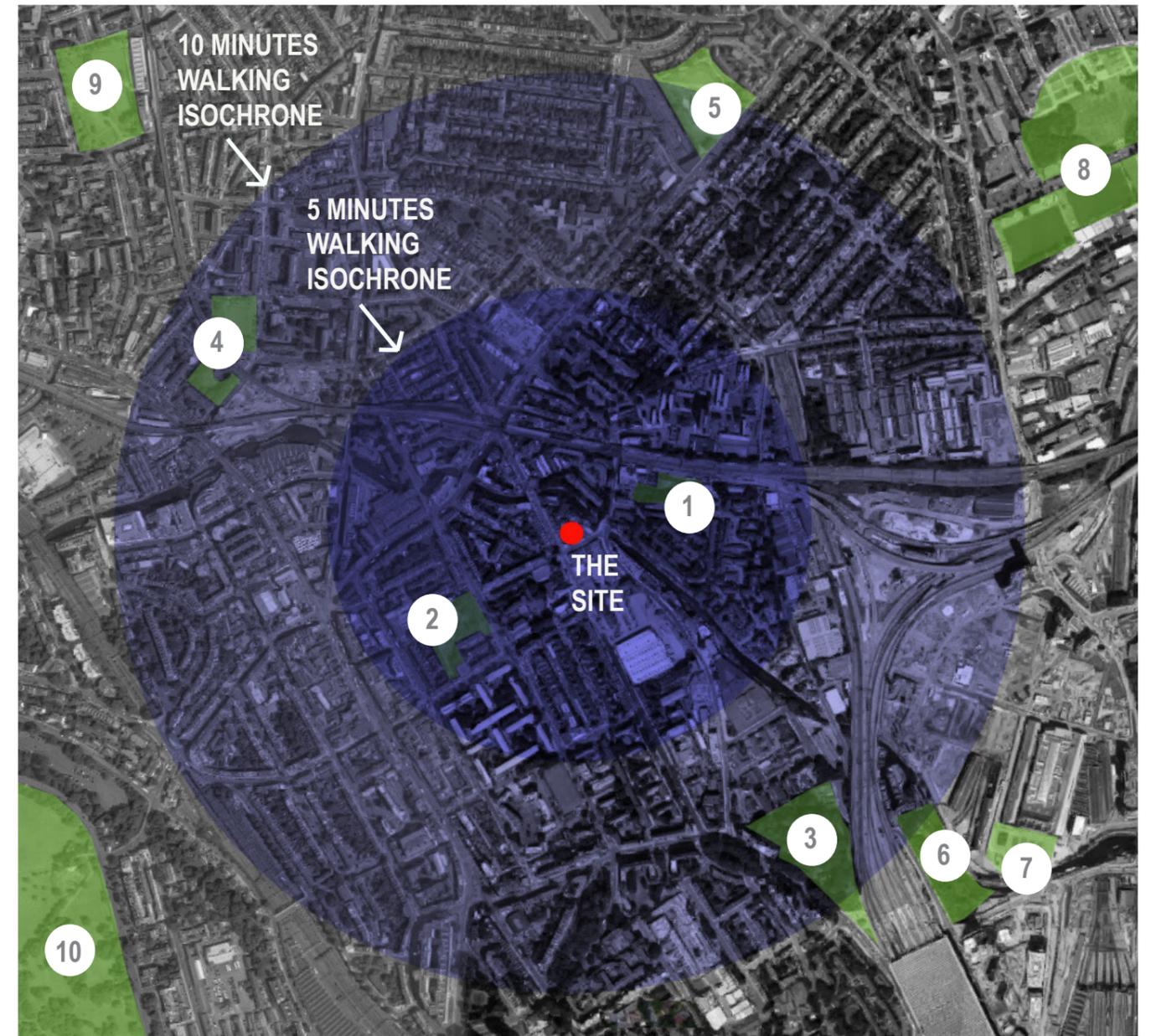
CPG2 requires provision of **private** outdoor amenity space for all apartments. In this scheme, private amenity is provided either as balconies or roof terraces which all meet, or exceed, the minimum areas required under the Mayor's Housing SPG and the requirements of CPG2, apart from those to the recessed section of the Georgiana Street frontage. As explained above, officers have agreed to relax the requirement for a minimum depth of 1.5m in this case, in order to achieve the required articulation to the frontage. The balconies are 1.5m<sup>2</sup> bigger than the required minimum however (6.5m<sup>2</sup> instead of 5m<sup>2</sup> for a 1 bed/2p apartment) due to their extended length.

CPG2 also requires provision of **communal** outdoor amenity space for residential development and refers to the Mayor's 2008 guidance. The latter has been replaced with the Mayor's latest 'Shaping Neighbourhoods: Play & Informal Recreation Supplementary Planning Guidance' 2012. CPG6 requires provision of new public open space for residential and for commercial developments which provide a net increase in accommodation. There is no increase in commercial space in this case and so the requirement applies to the new residential units only. The site does not fall within an area of Public Open Space Deficiency and is within 150m of Elm Village Open Space which includes play facilities for toddlers under 6, juniors 6-8 and older juniors 8-14 as well as the Urban Growth horticulture project.

The following pages describe the proposed landscape proposals for the scheme and how the requirements of CPG2 and CPG6 will be met.



Above: private amenity space will be provided as balconies or roof terraces



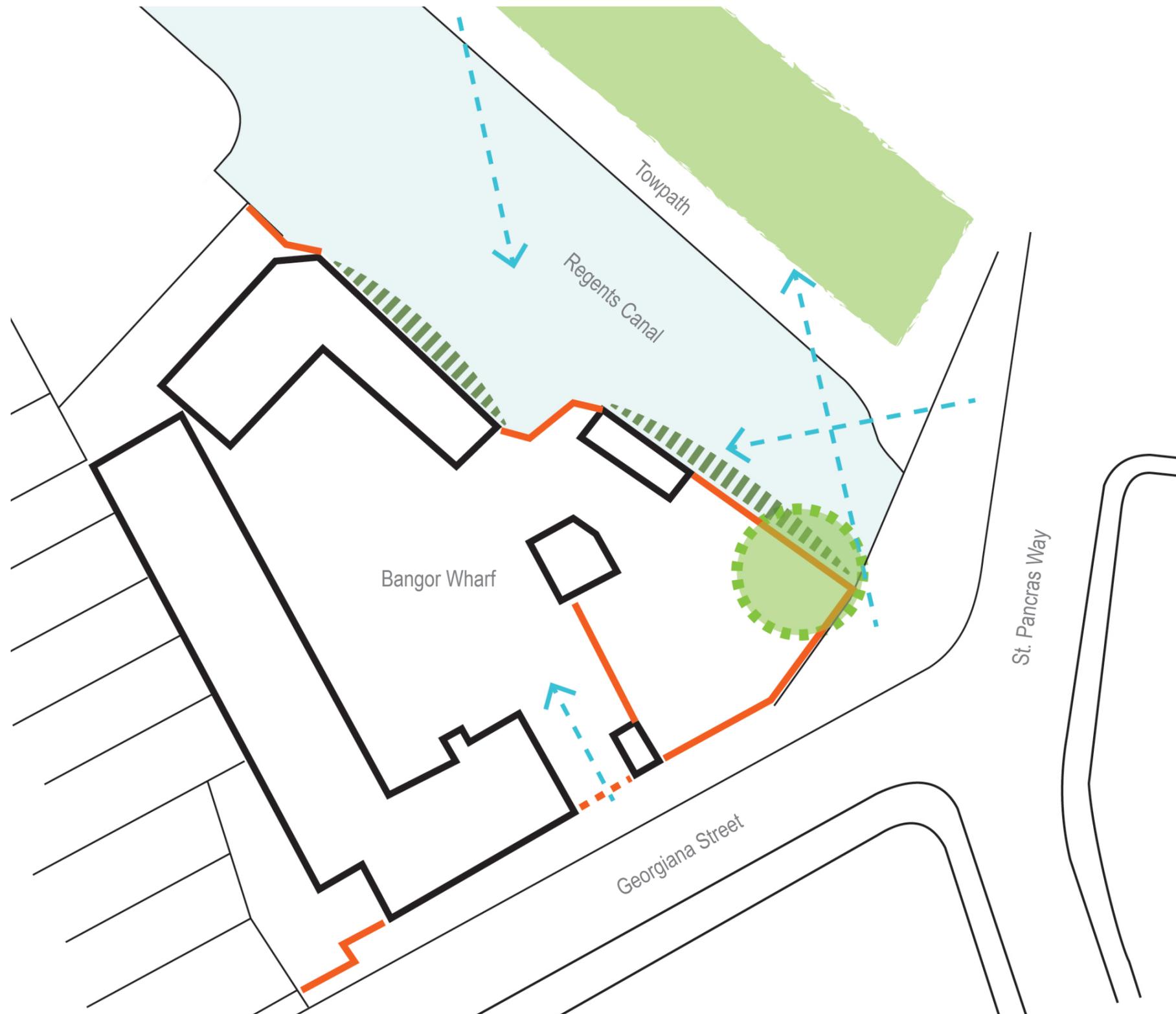
- Key to Isochrone Diagram and open space & recreation provision:
- 1 Elm Village Open Space
  - 2 St Martins Gardens
  - 3 St Pancras Churchyard Gardens
  - 4 Castlehaven Open Space
  - 5 Canteloves Skatepark
  - 6 Camley Street Natural Park
  - 7 Granary Square
  - 8 Caledonian Park & Tennis Centre
  - 9 Talacre Gardens & Community Sports Centre
  - 10 Regents Park



Above: play facilities for children at the Elm Village Open Space located a few minutes away from the site

## 5.00 Landscape design

## Existing Site



The site, Bangor Wharf, sits on the west side of Regents Canal, north of Georgiana Street. Access into the site is from Georgiana Street.

The site has no positive connection or relationship with the canal edge, a series of buildings, walls and palisade fences create a physical boundary between the canal edge and the internal courtyard. The view from the towpath, opposite, looking toward the site is poor and does little to contribute to the local amenity value; both visual or physical. An existing willow tree at the corner of the site and the St. Pancras Road bridge offers limited amenity value, but it is in poor condition and any works to the site suggest this would be significantly impacted.

Looking into the site from Georgiana Street, it provides glimpses of the tree line on the opposite side of the canal, along the tow path edge, which provides some visual relief and creates an effective screen to the residential properties beyond.

There is a significant level change from the access at Georgiana Street down into the central courtyard.

-  Existing building
-  Brick wall / boundary
-  Existing tree
-  Canal side vegetation
-  Views
-  Existing tree line



view looking from Regents Canal towpath towards the site; poor view of walls and palisade fencing with no active / positive relationship to the canal edge



view towards adjacent property on the north, where an external terrace area creates interest to the canal edge



looking into the site from Georgiana Street, with views of the trees on the opposite side of the canal



looking along Georgiana Street from St. Pancras Way; a mix of blank facades and site access turns its back on any relationship with the canal



looking into the site from bridge over St. Pancras Way, shows possible courtyard connection with canal edge

## Proposed Site Considerations

### Access



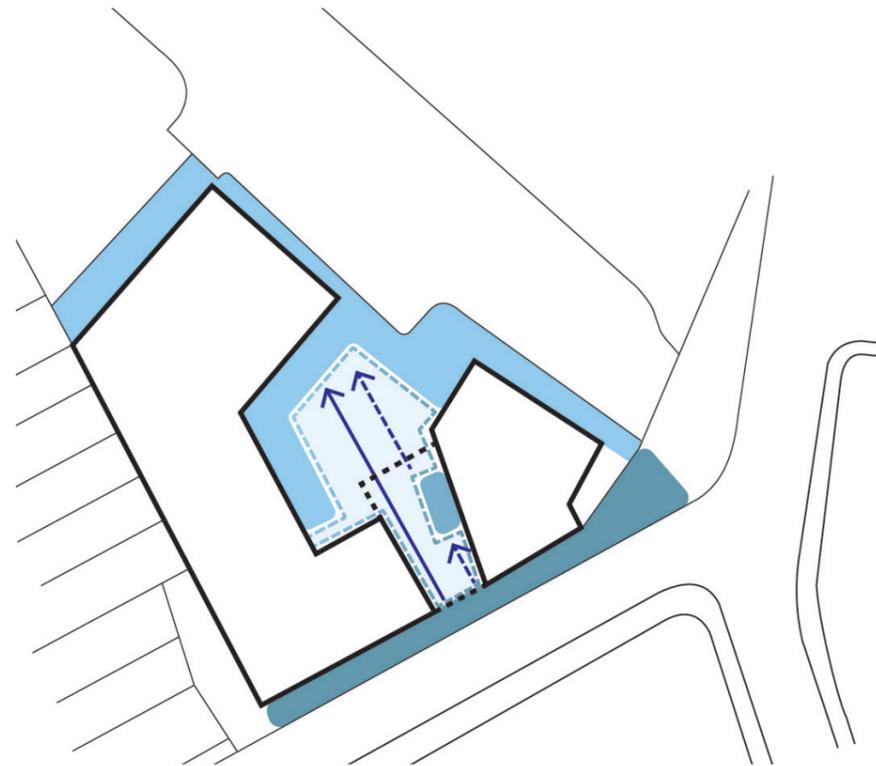
-  Commercial unit access
-  Residential access
-  Cycle parking / storage
-  Refuse access

Access to commercial and residential units is provided directly from Georgiana Street, with entry through to the courtyard and canal edge through a ground to first floor aperture in the building, which also facilitates cycle parking and refuse collection.

The courtyard will be open during daylight hours with restricted entry for tenants and residents outside of daylight hours.

A residential mooring may be facilitated along the canal edge, with access to the surrounding area via the courtyard. There is no public access along the towpath on the western edge of the canal.

### Levels

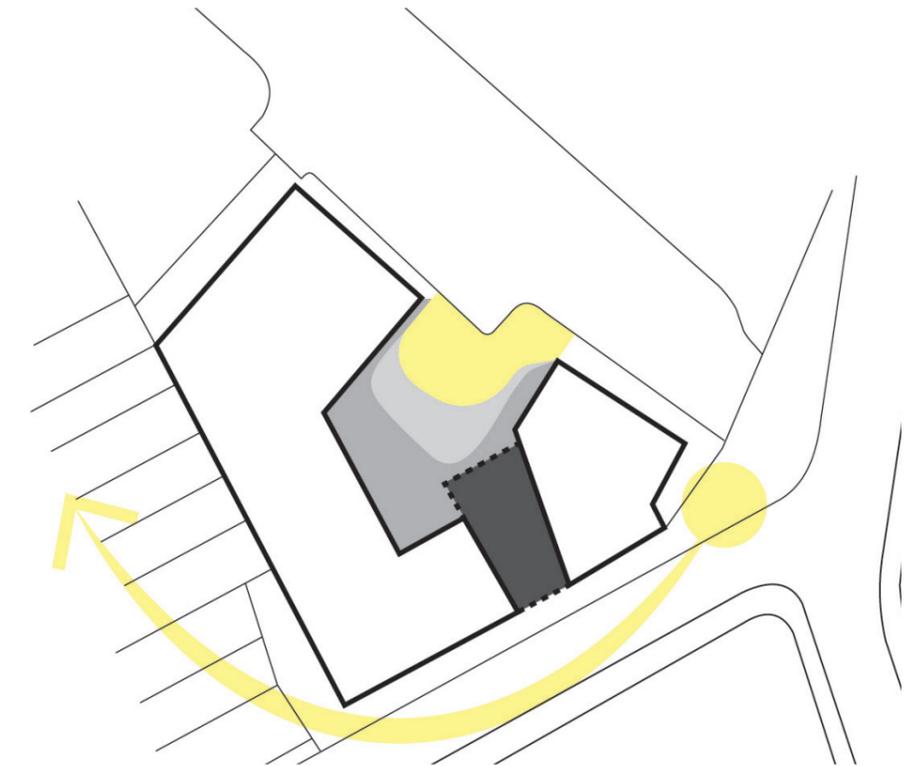


-  High - Georgiana Street
-  Intermediate - Residential Access
-  Transition
-  Low - Canal Edge

The levels generally fall from the main access along Georgiana Street towards the canal edge. A residential access core and refuse collection is located along the facade of building ..., which are stepped in height to allow a continuous fall across the courtyard.

The courtyard levels will fall from south to north, providing a positive relationship with the canal edge.

### Sun and Shade

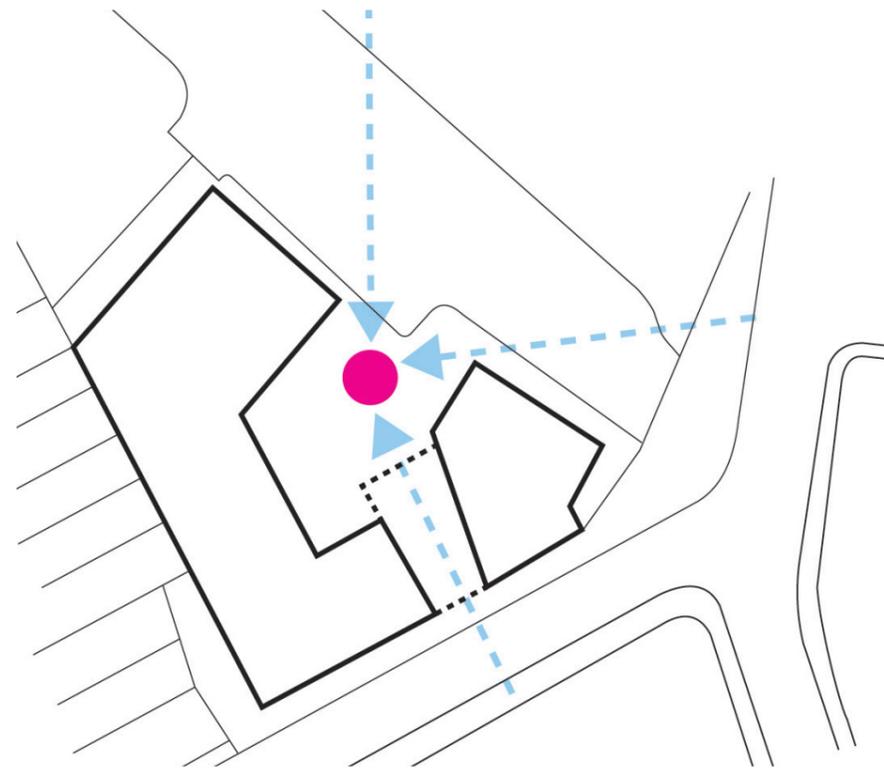


-  Sunniest
- 
- 
-  Shadiest

The building arrangement provides a positive, sunny relationship between the courtyard and the canal edge and water.

Shadier locations adjacent to the buildings will reinforce the centre of the courtyard and canal edge as the key points for activity.

## Views In



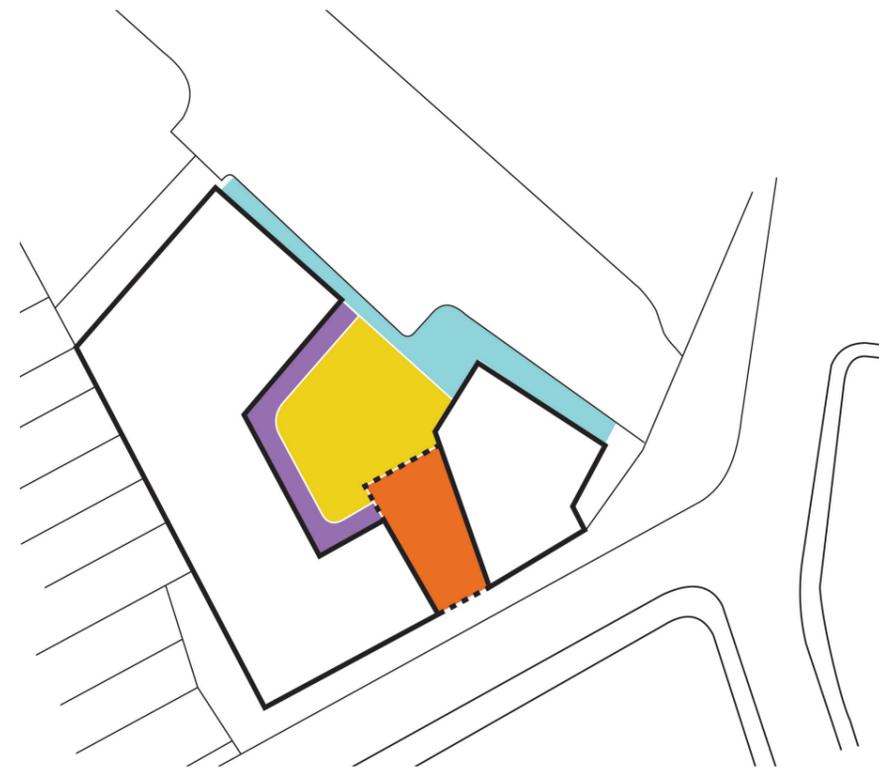
-  Key views in
-  Focal point

The courtyard entrance from Georgiana Street will be framed by the building aperture, framing views towards the canal.

There will be elevated views from the bridge of St. Pancras Way, as it crosses over the Regents Canal. The public towpath, on the opposite side of the development, will also provide views into the courtyard, which currently look on to a dilapidated brick wall.

The building arrangement creates a positive relationship for users to access the waters edge and experience new views of the canal looking out from the courtyard.

## Character

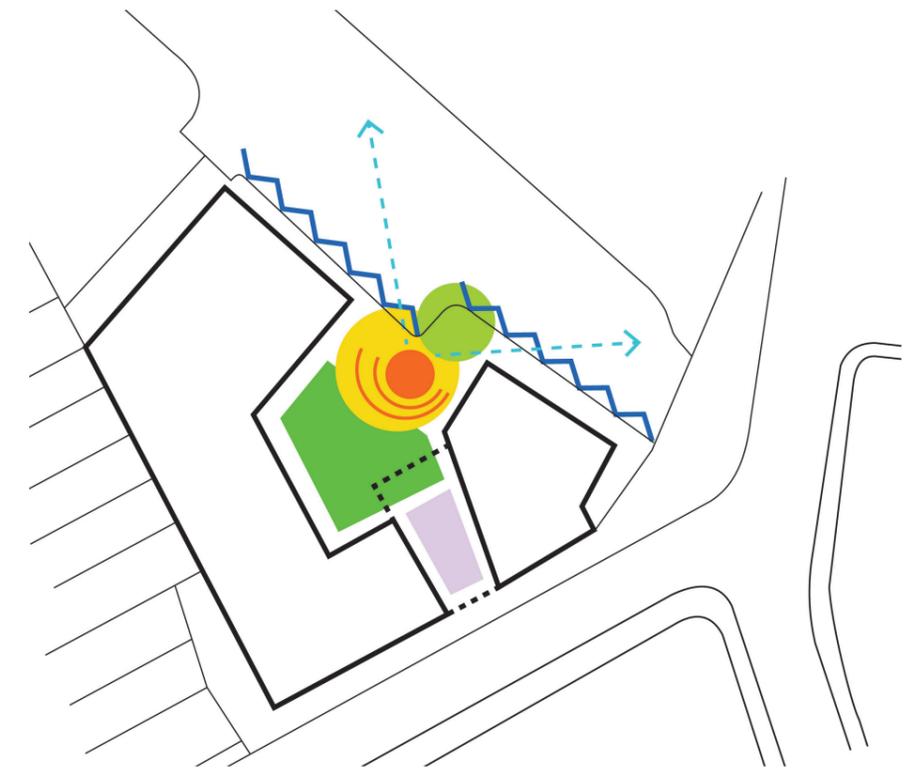


-  Arrival space
-  Central courtyard
-  Threshold space
-  Canal edge

An arrival space is created by the building aperture, creating a distinct transition from the adjacent highways to a courtyard setting. The central courtyard is framed by the building on three sides, promoting a positive relationship with the canal to the east.

Public access to the canal edge will be re-opened as an extension of the courtyard, providing what could be a quiet and serene space for visitors, tenants and residents.

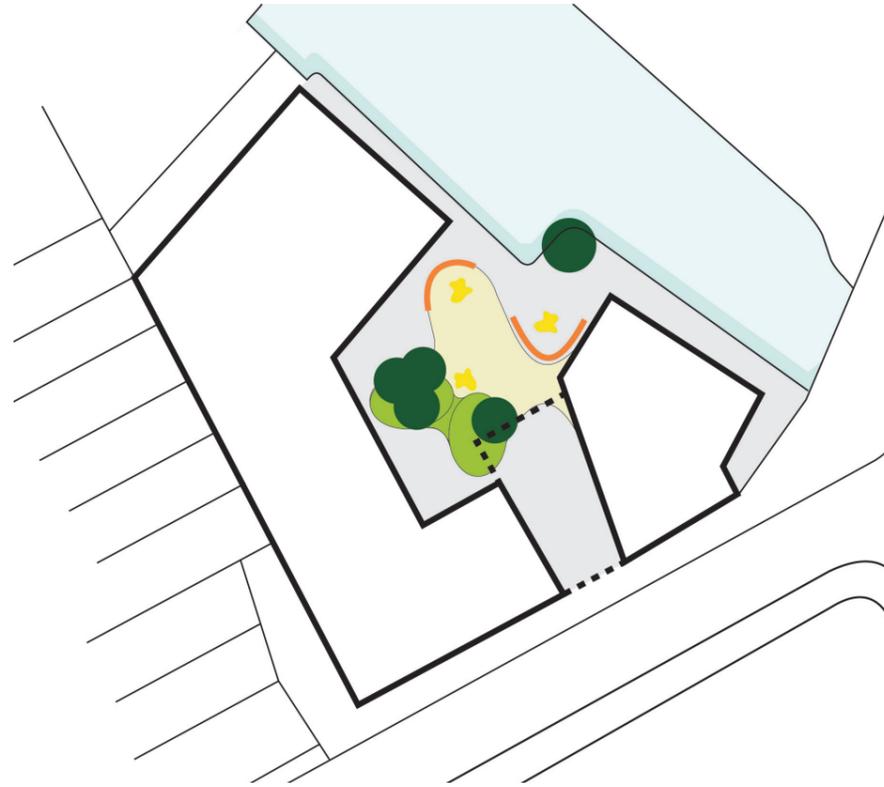
## Opportunities



-  Provide access to canal edge
-  Maximise sunny location
-  Provide opportunities to sit as part of level change
-  Maximise views looking out
-  Provide new feature tree to act as focal point
-  Provide new garden / courtyard setting
-  Celebrate arrival / transition

## Design Approach

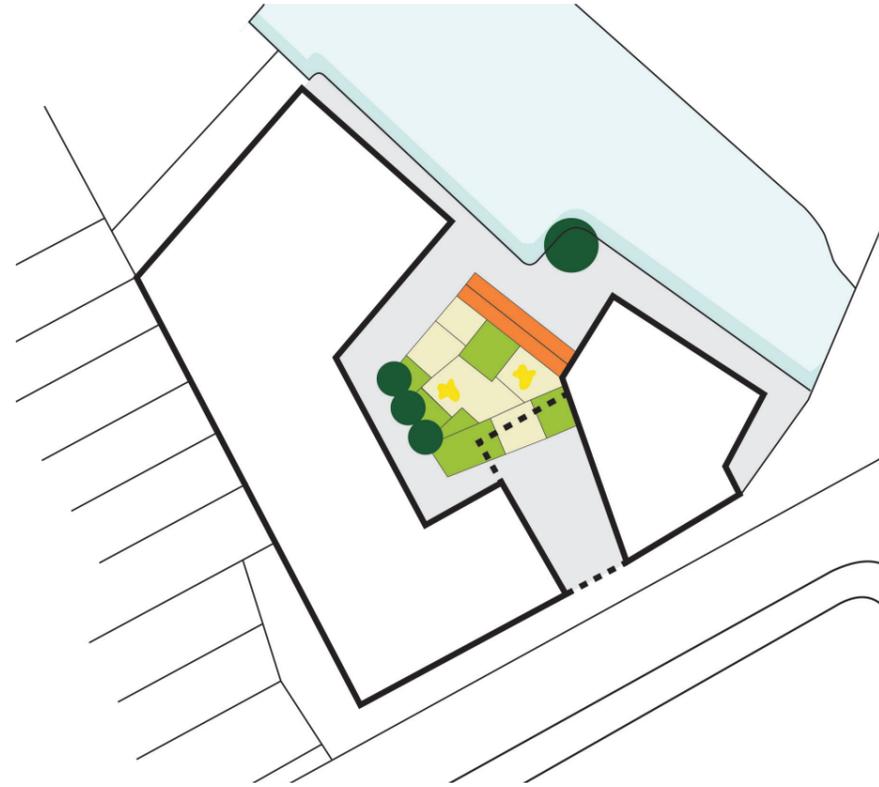
### Serpentine Courtyard



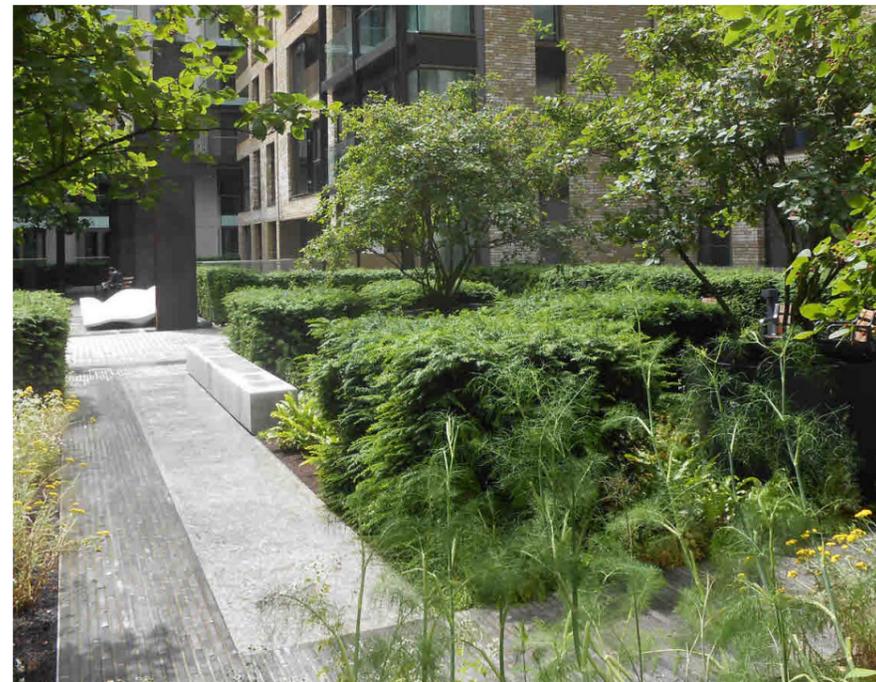
A free flowing arrangement of interconnecting spaces that create inward and outward looking spaces.



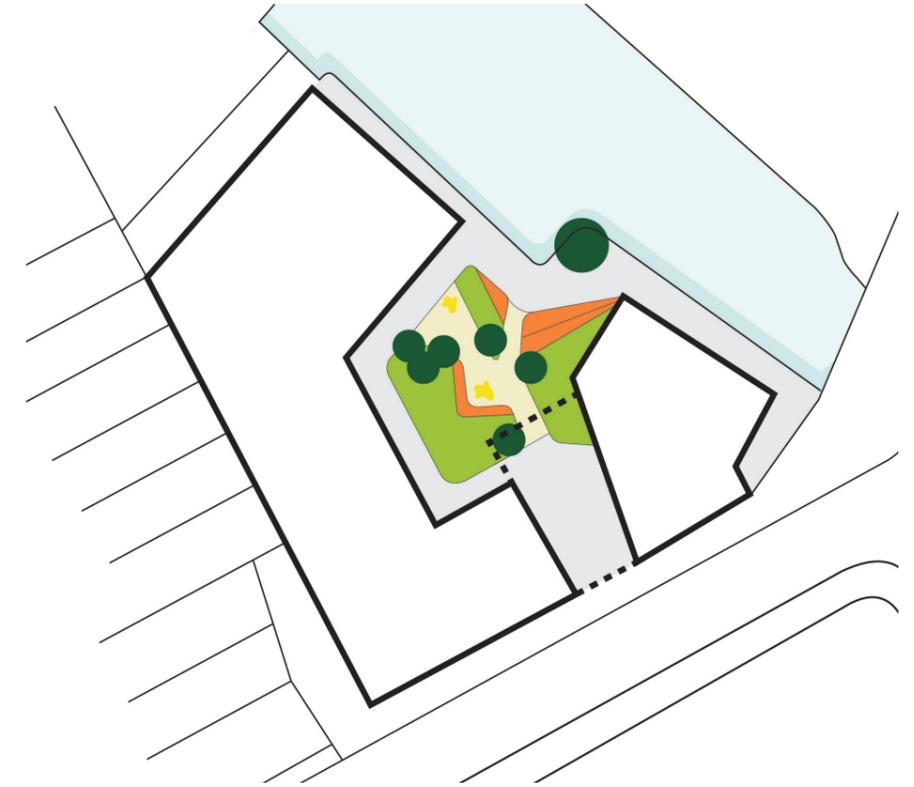
### Parterre Garden



An orthogonal arrangement of component parts, which can be further divided to provide different areas and outdoor rooms



### Contemporary Garden



An informal arrangement of spaces encapsulated within planted areas providing a contrast to ordered environment of the architecture.



## Design Approach

The design approach explored different themes to the courtyard for development with the client and design team.

The Serpentine approach looked at opportunities to break away from the architectural grid and create its own composition of bold forms, offering wide open views to the canal, as well as enclosed, inward looking social seating areas, with a planted, garden area providing a backdrop to the courtyard.

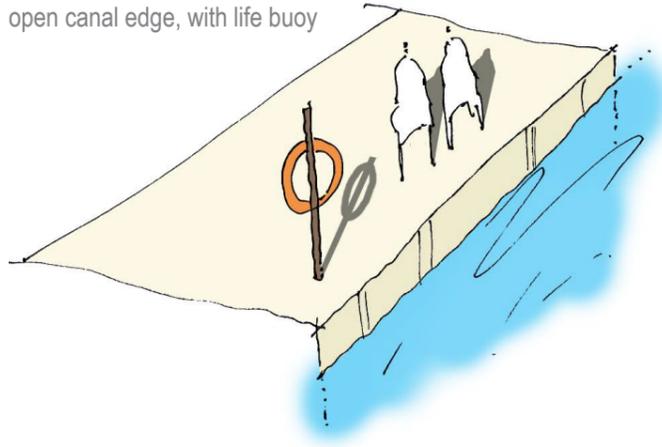
The Parterre Garden was a response to the building layout that could help to express the facades and elevational treatments. The ordered and controlled geometry of the planting would create a series of outdoor rooms and flexible space for tenants and residents, with a strong frontage to the canal.

The Contemporary Garden would create a central enclosed seating area set within a protective green edge, offering glimpses and views of the canal through planting and multi-stem shrubs. Broad seating areas open out on to the canal to maximise the sunniest area.

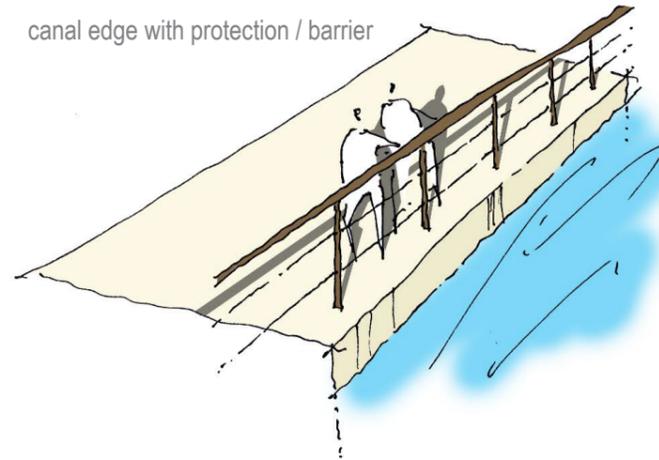
In all instances a new feature tree was proposed close to the canal edge to act as a focal point and help mitigate the loss of the existing willow tree closest to the bridge.

A sketch study of the canal edge options explored how open or protected the canal edge could be. The regeneration of the Bangor Wharf site will offer public access to the canal in this location which suggests that the treatment should be open, with some degree of safety.

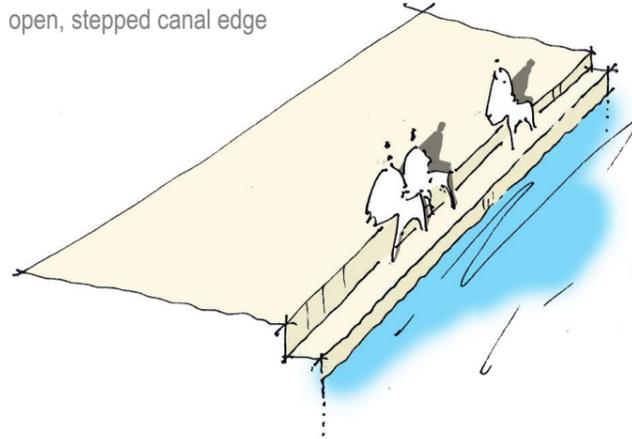
open canal edge, with life buoy



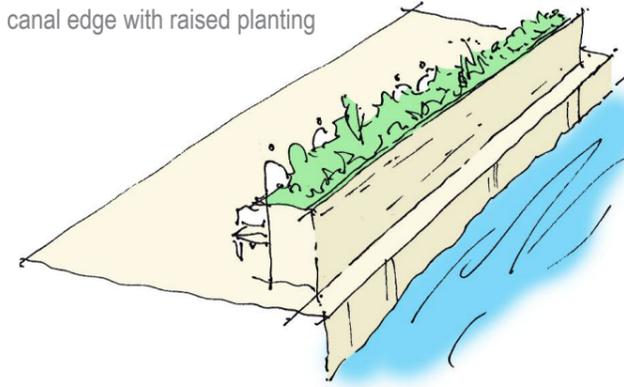
canal edge with protection / barrier



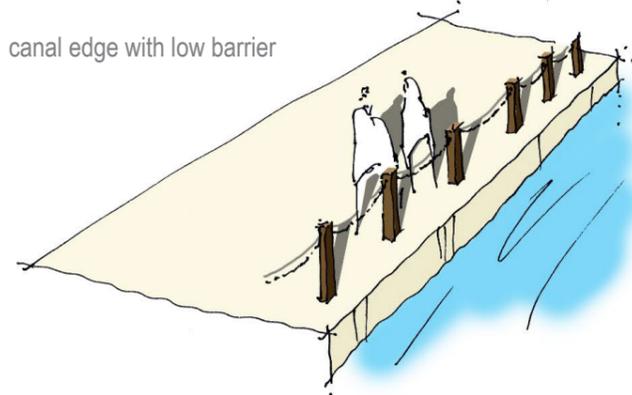
open, stepped canal edge



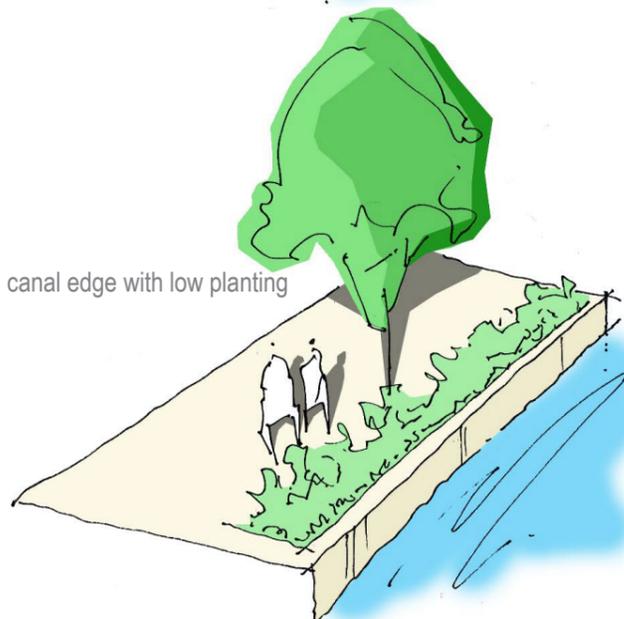
canal edge with raised planting

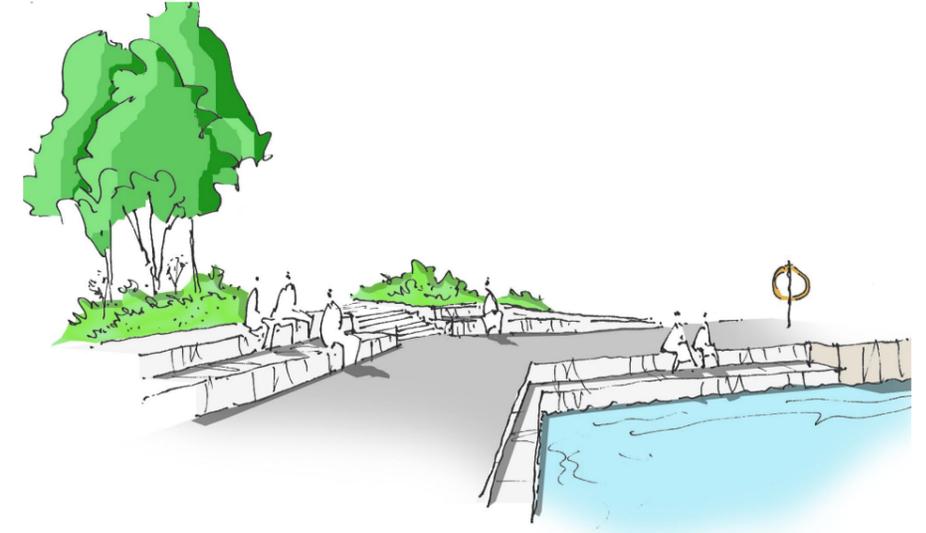


canal edge with low barrier



canal edge with low planting





Following feedback from the local authority, the client and design team, a combination of the Parterre and Contemporary Garden approach was developed to create a more industrial approach that was deemed more suitable to the canal side space. It was also agreed that an open canal edge would be more favourable and in keeping with the treatment more typical along Regents Canal.



The landscape design is intended to reflect the historical industrial nature of the site and maximise the canal side setting.

A new central courtyard is created by the arrangement of the buildings that provides a positive relationship to the canal, canal edge and Georgiana Street. The courtyard provides opportunities to sit and relax close to the canal edge, with a garden type environment providing a backdrop and screening to the groundfloor uses.

Access into the courtyard is from Georgiana Street, through an aperture in the ground floor of the building, providing views and glimpses towards the canal and courtyard. This access will be used by residents and commercial tenants and will be publicly accessible during daylight hours.

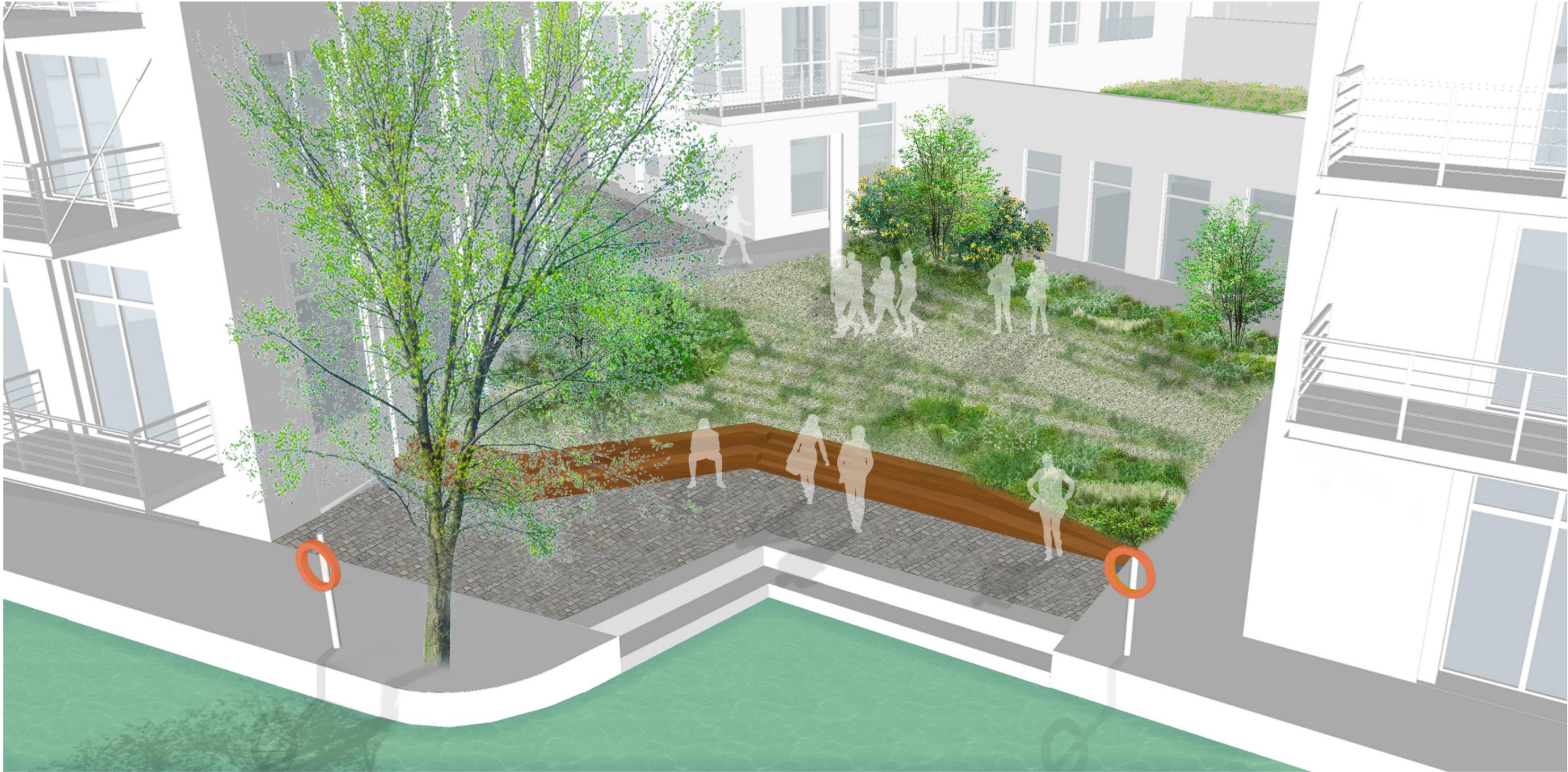
The canal edge is restored allowing people to get to close to the water with no physical barrier, in keeping with most canal side conditions along Regents Canal. A double stepped edge condition allows people to sit closer to the water in the sunniest location.

A private garden and break out space is provided at the rear of the ground floor commercial units of Block C, adjacent to the neighbouring terrace on the north. A raised planter with low cover and climbing wires in front of the boundary wall will create a pleasant backdrop to look out upon.

A green roof to Block C will provide visual interest for tenants to look down upon, and create a green visual connection for residents looking out from the adjacent Royal College Street, over the courtyard towards Regents Canal.

A residents mooring location, provided along the canal inlet, will be served by a tap.

1. Central courtyard
2. Resident / tenant access with Public access during daylight hours
3. Restored canal edge
4. Tenant garden and break out space
5. Green roof
6. Residents Mooring



## Landscape Design

### Courtyard Design

The arrival area into the central courtyard uses the reclaimed and salvaged granite setts from the existing site, providing a reference back to the industrial past of the site. The reclaimed setts will be flanked by sawn granite paving that will provide a smoother path for wheelchairs, buggies, refuse access etc. Visitor cycle stands are located closest to the courtyard entrance, with good views from adjacent ground floor uses.

At the heart of the courtyard a route is defined to block C by long granite planks, that start to radiate and fragment amongst a more informal gravel surface through which planting emerges. The areas of planting start to intensify around the edges to create a garden backdrop to courtyard and frame the seating areas, maximising views out towards the canal. Two feature benches are set amongst the central courtyard, which will be oversized see-saw benches, providing a playful landscape for children.

Steps from the courtyard and a building perimeter path lead visitors to a wide generous space adjacent to the canal in the sunniest location. Large timber seating steps create an edge to the garden area, looking out towards the canal. The canal edge in this location steps to provide further opportunities to sit in the sunny area, beneath a new feature Willow tree. The new tree will act as a focal point at this junction along the canal, for people looking in and users looking out. Life rings are provided along the canal edge, for use in emergencies. The life ring post located closest to Block B will also incorporate a new water tap for the residential mooring.

The paths along the canal edge and around the building threshold will be a sawn granite plank finish.



1. Arrival area
2. Central gravel space with plank paving
3. Timber step seating
4. Stepped canal edge
5. Feature tree
6. Seating and play
7. Cycle stands
8. Building threshold
9. Life ring



building threshold | garden backdrop to courtyard | route defined by granite plank paving | planted edge | canal side space with feature tree and timber seating edge | canal wall rebuilt

Section A-a



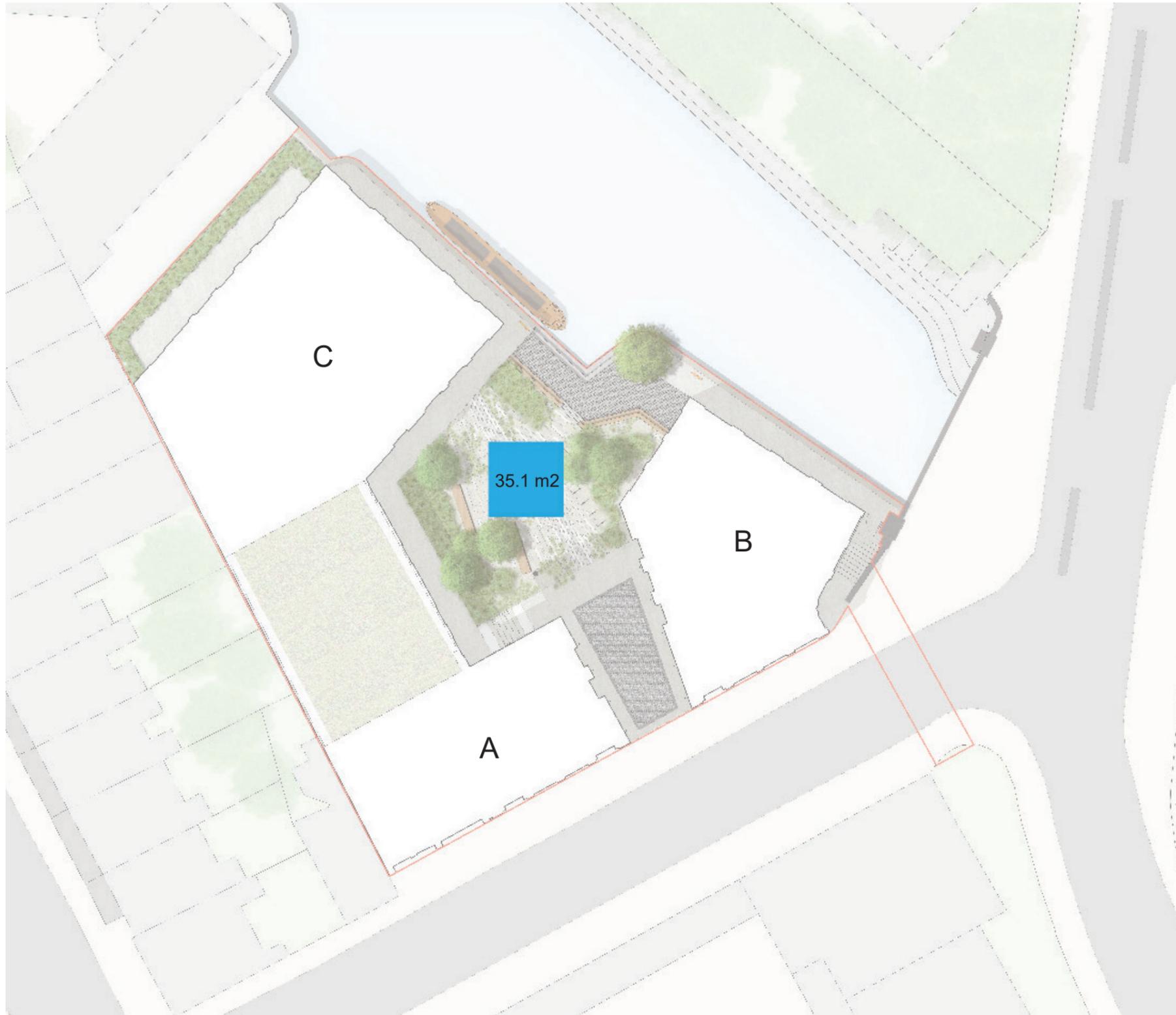
building threshold | garden backdrop to courtyard | route defined by granite plank paving | planted edge | canal side space stepped canal edge

Section B-b



building threshold | timber feature seat | garden backdrop to courtyard with feature shrubs | timber feature seat | feature shrub | building threshold

Section C-c



The development generates the need for 35.1m<sup>2</sup> of childrens playspace as detailed below.

Paragraph 11.23 of the Council's Planning Guidance 6 - Amenity - Open space, sport and recreation (attached) requires the following children's play space per sq.m;

- 1 bed unit - nil
- 2 bed unit - 0.6 m<sup>2</sup>
- 3 bed unit - 2.9 m<sup>2</sup>; and
- 4 bed unit - 3.6 m<sup>2</sup>.

The development provides;  
16 x 1 bed units  
15 x 2 bed units; and  
9 x 3 bed units

As such the proposal generates a total need for **35.1 m<sup>2</sup>** of children's playspace.

Playful see-saw benches are proposed to create a dynamic, interactive play feature that is in keeping with the industrial inspiration and character of the site. The courtyard also provides a range of playful and sensory opportunities beyond the required amount.





## Levels

Levels typically fall from the highest point; at the main access point on Georgiana Street, towards Block C at a constant gradient. Door thresholds and internal levels are adjusted to suit this level change along Block B where required.

A constant level is achieved around the threshold of Block C. A shallow steps with a bike runnel creates a direct connection between the cycle store provision, block C, to the main entrance. Alternative access to the cycle store can also be obtained via the sloped courtyard.

A set of steps connects the central courtyard with the canal side space, as does the level threshold outside of Block C. Larger seating steps utilise the level change between the canal edge and the central courtyard.

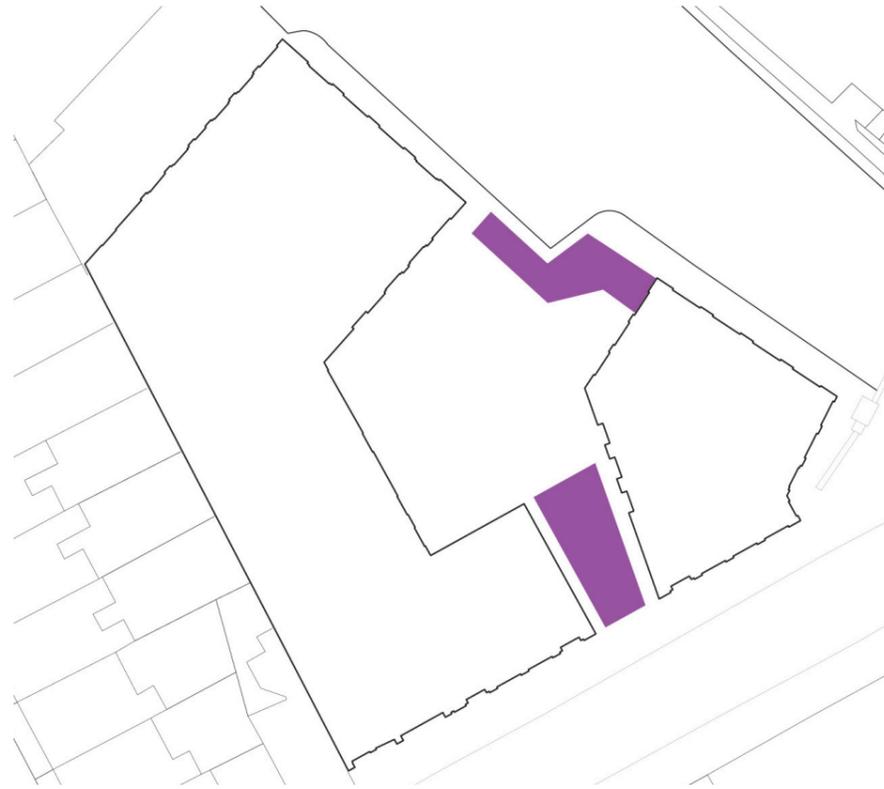
A constant level is achieved along the canal edge, with a double stepped seating area at the junction on the canal inlet.

Steps will have handrails and tactile paving where required.

- 1. Sloped entrance
- 2. Level threshold / canal edge
- 3. Access steps
- 4. Seating steps

## Materials

### Reclaimed granite cobbles



Two key areas; the arrival and canal side spaces, are paved using site reclaimed granite cobbles, providing a reference to the sites history and industrial past.



### Granite paving



Sawn granite plank paving to the building thresholds, connecting route through the courtyard and canal edge provide comfortable pedestrian routes.



### Central courtyard



The central courtyard will be a gravelled surface with granite planks, through which planting will grow.

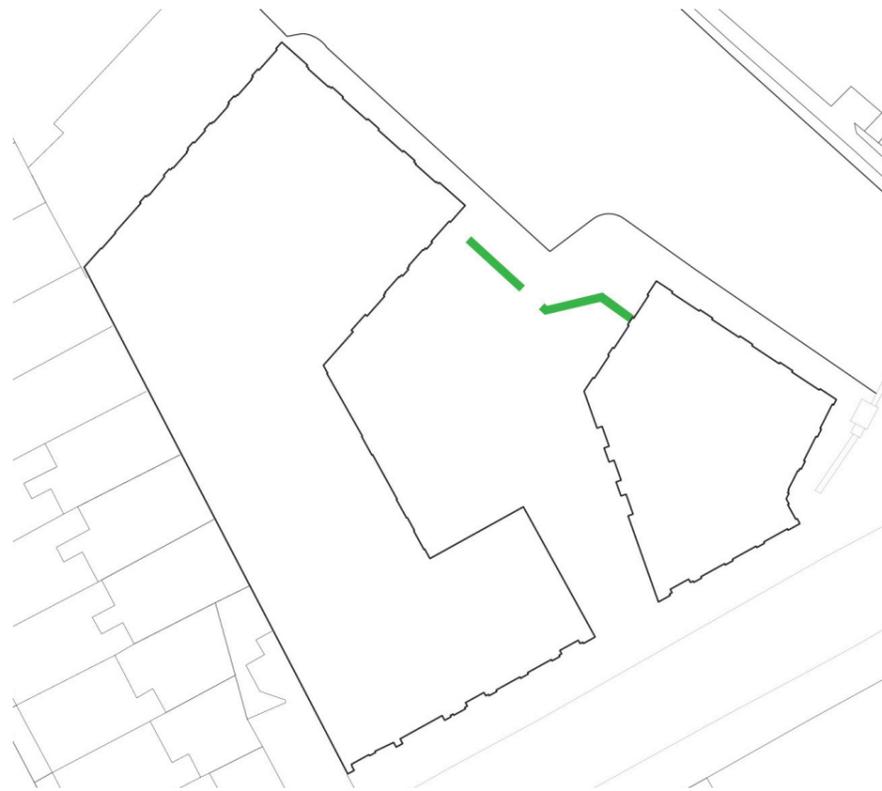


Feature benches



Two oversized benches set within the courtyard and garden look out towards the canal, will be playful see-saw features providing opportunities to sit alone or in groups.

Timber seating



Large timber seating steps provide opportunities to sit in the sunniest location of the courtyard, looking out towards the canal.

Canal edge

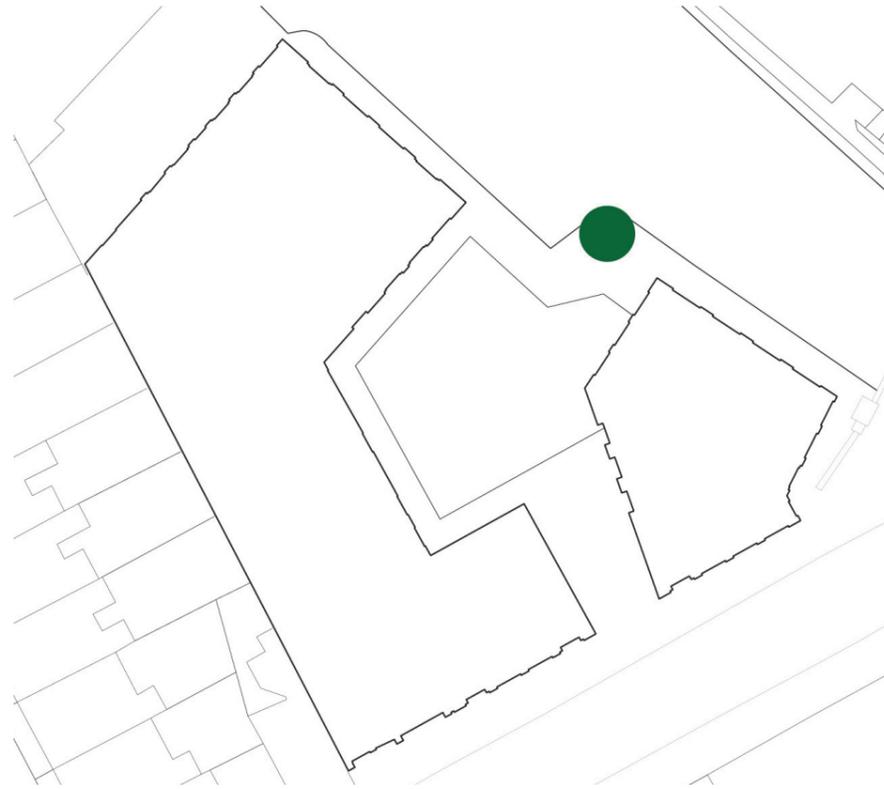


The canal wall is to be rebuilt, and part of this will include stepped edge to the new canal wall will allow people to sit closer towards the water



# Planting

## Feature tree



A new feature willow tree; *Salix alba*, will be located on the canal edge to provide a focal point and replace the visual amenity of the tree to be removed closer towards the bridge.



## Feature shrubs



Feature shrubs will be used to add height, frame views and provide dramatic seasonal impact and colour.



## Courtyard planting



Ferns and grasses will provide a contemporary, verdant backdrop to shady corners. Variation in height, texture and form will help to add year round evergreen interest.



Boundary planting



Climbing plants will be used along the northern boundary to create a green boundary.

Biodiverse roof



A biodiverse roof on Block C will help will help to improve peoples outlook from apartments above and will help to improve local biodiversity and ecology.



Planting

Species



Dryopteris afinis 'Crisata'



Dryopteris filix-mas

Anemone 'honorine jobert'

Amelanchier lamarckii

Asplenium scolopendrium

Cornus alba 'Aurea'

Seasonal timetable

Ultimate Height (m)

FEB MAR APR MAY JUN JUL AUG SEPT OCT NOV DEC JAN

*Anemone 'honorine jobert'*

1-1.5



*Amelanchier lamarckii*

8-12



*Asplenium scolopendrium*

0.1-0.5



*Cornus alba 'Aurea'*

1.5-2.5



*Dryopteris afinis 'Crisata'*

1-1.5



*Dryopteris filix-mas*

1-1.5





Deschampsia ces.  
'Bronzeschleier'



Escallonia 'Donard Radiance'



Liriope muscari



Luzula sylvatica



Matteuccia struthiopteris



Sarcococca hookeriana var humilis

**Seasonal timetable**

Ultimate Height (m)

FEB      MAR      APR      MAY      JUN      JUL      AUG      SEPT      OCT      NOV      DEC      JAN

*Deschampsia ces.*  
'Bronzeschleier'

0.5-1



*Escallonia* 'Donard Radiance'

1.5-2.5



*Liriope muscari*

0.1-0.5



*Luzula sylvatica*

0.5-1



*Matteuccia struthiopteris*

1-1.5



*Sarcococca hookeriana*  
'Humilis'

0.5-1



## 5.00 Access Statement

### 5.01 Public transport

The site has excellent public transport links with both Camden Road Station (London Overground) within 5 minutes' walk and Camden Town Station (London Underground) within 10 minutes' walk. There are numerous bus stops within a few minutes' walk of the site with routes into inner and outer London.

The PTAL score for the site is **6a**.

The site also has easy access to the canal towpath, providing excellent pedestrian and cycle connections to Kings Cross, Camden Town and beyond.

### 5.02 Local facilities

The area is very well-served for local facilities. Camden High Street, Camden Lock and Inverness Street markets are all within 5-10 minutes' walk. There are a number of primary schools within a short walk, Camden School for Girls is approx 0.9km to the north-east and Haverstock School is approx 1.5km to the north-west. There are two small parks with playgrounds nearby, Regents Park and Zoo is a short bus ride away and there are three sports centres in the vicinity.

### 5.03 Vehicle and bicycle access

The scheme is proposed as car-free, given its town centre location and excellent public transport links.

Following pre-application discussions with Camden's Transport Planners, it has been agreed that servicing can take place from the street and that Georgiana Street has capacity for wheelchair parking spaces on-street. Refer to Transport Statement for more details.

The site has access to many of Camden's dedicated cycle routes and the use of cycles will be encouraged through the provision of bike storage as follows:

- a) for the residential apartments with a total of 64 spaces provided within secure, internal cycle stores in accordance with the latest London Plan requirements (see right).
- b) for the B1 commercial space with a total of 10 spaces provided within secure, internal cycle stores in accordance with the latest London Plan requirements (1 space/90m).
- c) for an additional 4 spaces provided externally by 2 no. Sheffield stands for visitors in the courtyard (see Landscape plan for locations).

All spaces are conveniently accessed at ground level.



#### RESIDENTIAL CYCLE PARKING

| BUILDING A                      | No Units  | Cycle spaces/unit | TOTAL     |
|---------------------------------|-----------|-------------------|-----------|
| 1 BEDS                          | 0         | 1                 | 0         |
| 2 BEDS                          | 2         | 2                 | 4         |
| 3 BEDS                          | 4         | 2                 | 8         |
| <b>TOTAL</b>                    | <b>6</b>  |                   | <b>12</b> |
| BUILDINGS B & C                 | No Units  | Cycle spaces/unit | TOTAL     |
| 1 BEDS                          | 16        | 1                 | 16        |
| 2 BEDS                          | 13        | 2                 | 26        |
| 3 BEDS                          | 5         | 2                 | 10        |
| <b>TOTAL</b>                    | <b>34</b> |                   | <b>52</b> |
| <b>TOTAL SPACES RESIDENTIAL</b> |           |                   | <b>64</b> |

|                        |                         |          |
|------------------------|-------------------------|----------|
| <b>VISITOR PARKING</b> | <b>1 SPACE/40 UNITS</b> | <b>1</b> |
|------------------------|-------------------------|----------|



## 5.04 Inclusive access

Inclusive design is about making places that everyone can use. For a scheme to be inclusive, it needs to be carefully considered from the outset so the buildings will be:

- flexible: so different people can use them in different ways
- convenient: so everyone can use them without too much effort or separation
- accommodating: for all people, regardless of their age, gender, mobility, ethnicity or circumstances
- welcoming: with no disabling barriers that might exclude some people
- legible: so everyone knows where they are and can easily locate their destination

The proposed buildings will do all these things.

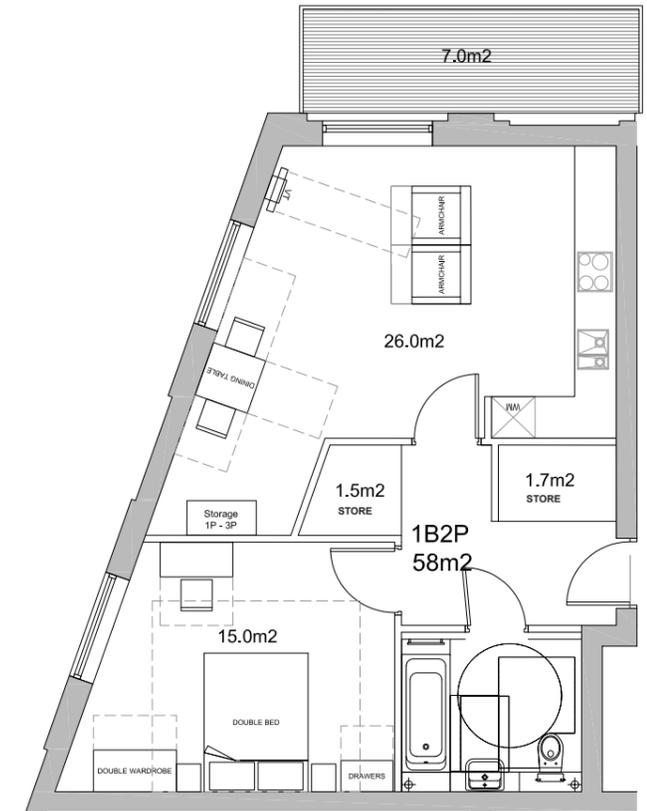
They are arranged so that everyone will be able to access all parts of the scheme without hindrance or inconvenience. The buildings will be legible and entrances will be conveniently placed and easily visible. The public space and shared amenity space will also be secure, overlooked and safe.

The external approach to the site from Camden Town is generally level but will slope down after entering the site. The ground will be graded to ensure that falls are max 1:20 across the courtyard and that access can be provided to all areas without using steps.

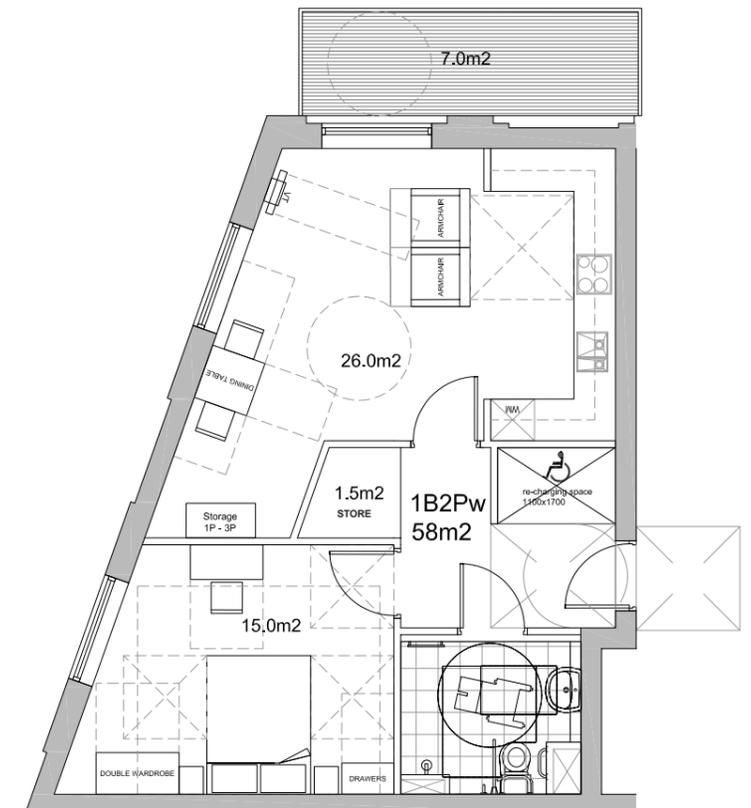
External areas will be well lit and designed to prevent glare or shadows. Entrances will be sheltered from rain and will have doors that provide minimum 900mm clear opening to allow both wheelchairs and double buggies. Entrance doors will have glazed panels providing a zone of visibility from 900-1500mm (min) from ffl.

Residential entrances are served by Part M passenger lifts, providing access to all floors. All new homes are designed to meet Part M4(2) standards, allowing adaptability for future needs. Four apartments in Building C are designed to be adaptable to meet Part M4(3) if required. The adaptable and adapted layouts are shown, right.

The B1 units at ground floor are all designed to be step-free and with level thresholds.



Above: apartment layout as easily adaptable to meet full wheelchair to Part M4(3)  
Below: same apartment layout as adapted to meet Part M4(3)



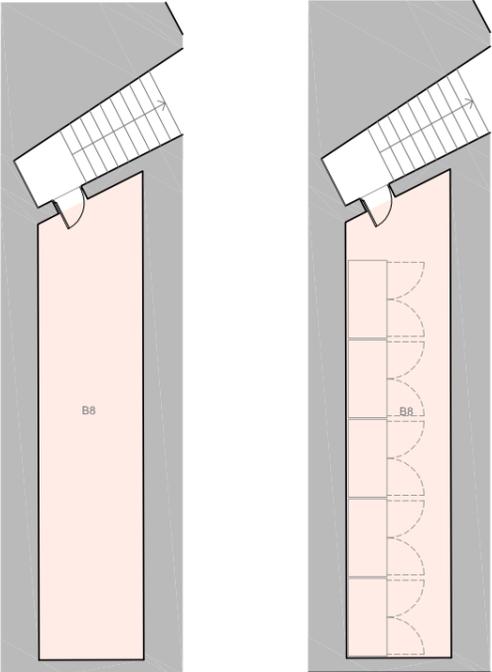
# Appendices

# Appendix 1: Flexible workspace

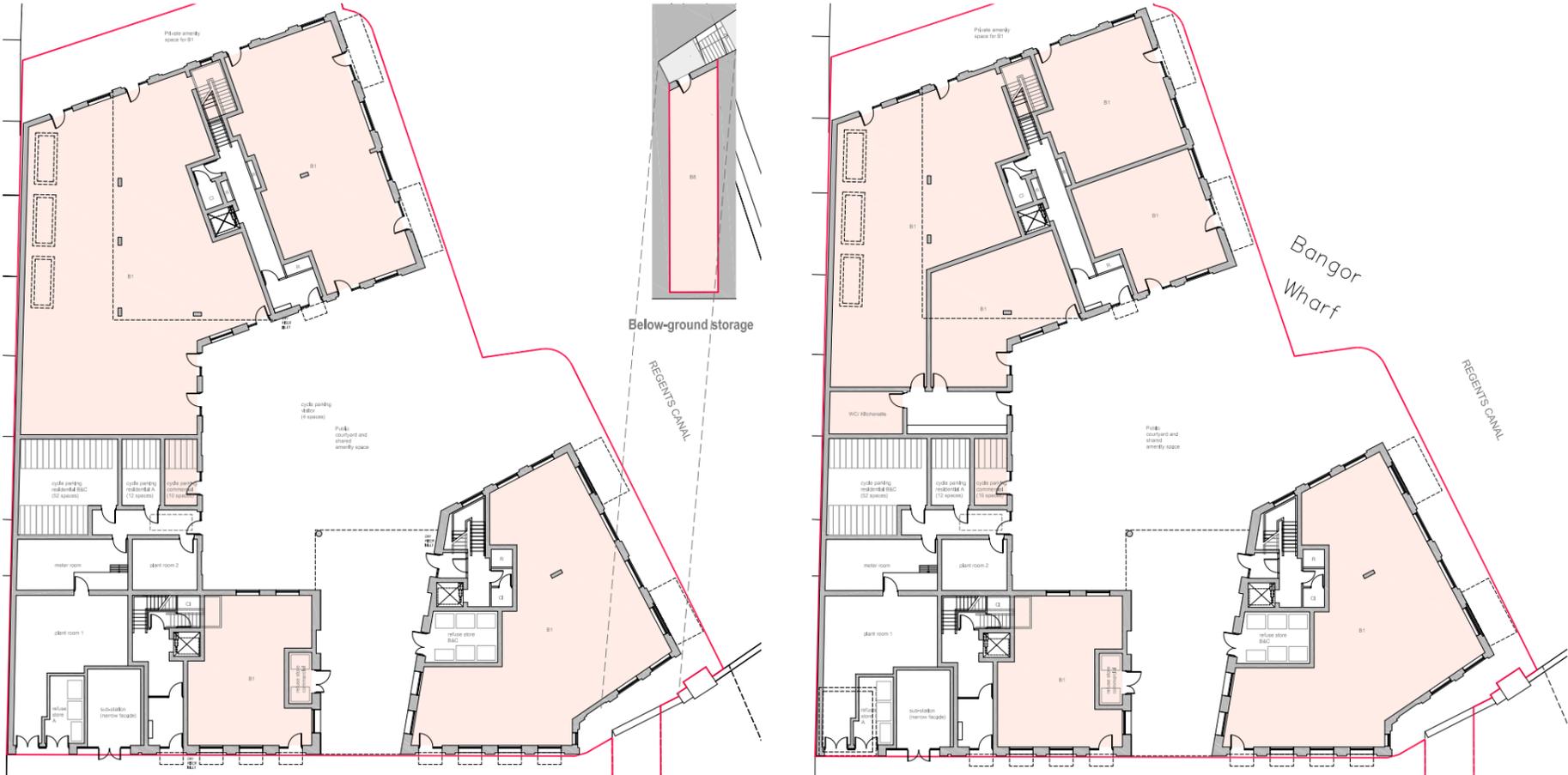
The proposal for the new B1 accommodation at ground floor is designed to be flexible. The application plans show the space available as three separate, open-plan units. The plans (right) indicate a way of subdividing this space to provide smaller units, for start-ups and SME's, if that is required.

The B8 accommodation is located in the existing tunnel under the road and will be accessed via a gate and shallow steps. It will be offered as ancillary space to the B1.

Right: B8 storage space provided in existing tunnel under the street, shown lettable as one space (left) or fitted with individual storage units for letting to different B1 units (right)



Below: B1 space at ground floor, showing a variety of sizes of units (left) or sub-divided to provide smaller units for start-ups or SME's (right)



## Appendix 2: Dwelling sizes

The table (right) shows the breakdown of the accommodation schedule shown on page 26 above with individual dwelling sizes.

These all meet the Technical Housing Standards - Nationally Described Space Standards March 2015 published by DCLG and the Mayor's Housing SPG 2016.

## Schedule of accommodation: block by block

|                      |                       | 1 BED/2P   | 2BED/3P | 2 BED/4P | 3 BED/4P | 3 BED/5P |       | 1 BED/2P    | 2BED/3P | 2 BED/4P | 3BED/5P |     |       |
|----------------------|-----------------------|------------|---------|----------|----------|----------|-------|-------------|---------|----------|---------|-----|-------|
|                      |                       | UNIT AREAS |         |          |          |          |       | UNIT AREAS  |         |          |         |     |       |
| BLOCK A (SOUTH WEST) |                       | AFF RENT   |         |          |          |          | TOTAL | SOCIAL RENT |         |          |         |     | TOTAL |
|                      |                       | sqm        |         |          |          |          |       |             |         |          |         |     |       |
| 1                    | UNIT A.G01_WHEELCHAIR | G FLOOR    |         |          |          |          |       |             |         |          |         |     |       |
| 2                    | UNIT A.101            | 1st FLOOR  |         |          | 73       |          |       |             |         |          |         |     |       |
| 3                    | UNIT A.102            |            |         |          |          |          |       |             |         |          | 89      |     |       |
| 4                    | UNIT A.201            | 2nd FLOOR  |         |          | 73       |          |       |             |         |          |         |     |       |
| 5                    | UNIT A.202            |            |         |          |          |          |       |             |         |          | 89      |     |       |
| 6                    | UNIT A.301            | 3rd FLOOR  |         |          |          | 84       |       |             |         |          |         |     |       |
| 7                    | UNIT A.302            |            |         |          |          |          |       |             |         |          |         |     |       |
| 8                    | UNIT A.401            | 4th FLOOR  |         |          |          | 84       |       |             |         |          |         |     |       |
| 9                    | UNIT A.402            |            |         |          |          |          |       |             |         |          |         |     |       |
| <b>GIA</b>           |                       |            | 0       | 0        | 146      | 168      | 0     | 314         | 0       | 0        | 0       | 178 | 178   |
| <b>No UNITS</b>      |                       |            | 0       | 0        | 2        | 2        | 0     | 4           | 0       | 0        | 0       | 2   | 2     |

|                      |            | 1 BED/2P   | 2BED/3P | 2 BED/4P | 3 BED/4P | 3 BED/5P |       | 1 BED/2P     | 2 BED/3P | 2 BED/4P | 3BED/5P |   |       |
|----------------------|------------|------------|---------|----------|----------|----------|-------|--------------|----------|----------|---------|---|-------|
|                      |            | UNIT AREAS |         |          |          |          |       | UNIT AREAS   |          |          |         |   |       |
| BLOCK B (SOUTH EAST) |            | PRIVATE    |         |          |          |          | TOTAL | INTERMEDIATE |          |          |         |   | TOTAL |
|                      |            | sqm        |         |          |          |          |       |              |          |          |         |   |       |
| 11                   | UNIT B.101 | 1st FLOOR  |         |          | 82       |          |       |              |          |          |         |   |       |
| 12                   | UNIT B.102 |            |         |          |          |          |       |              |          |          | 92      |   |       |
| 13                   | UNIT B.103 |            | 50      |          |          |          |       |              |          |          |         |   |       |
| 14                   | UNIT B.104 |            | 51      |          |          |          |       |              |          |          |         |   |       |
| 15                   | UNIT B.201 | 2nd FLOOR  |         |          | 82       |          |       |              |          |          |         |   |       |
| 16                   | UNIT B.202 |            |         |          |          |          |       |              |          |          | 92      |   |       |
| 17                   | UNIT B.203 |            | 50      |          |          |          |       |              |          |          |         |   |       |
| 18                   | UNIT B.204 |            | 51      |          |          |          |       |              |          |          |         |   |       |
| 19                   | UNIT B.301 | 3rd FLOOR  |         |          | 82       |          |       |              |          |          |         |   |       |
| 20                   | UNIT B.302 |            |         |          |          |          |       |              |          |          | 92      |   |       |
| 21                   | UNIT B.303 |            | 50      |          |          |          |       |              |          |          |         |   |       |
| 22                   | UNIT B.304 |            | 51      |          |          |          |       |              |          |          |         |   |       |
| 23                   | UNIT B.401 | 4th FLOOR  |         |          | 82       |          |       |              |          |          |         |   |       |
| 24                   | UNIT B.402 |            |         |          |          |          |       |              |          |          | 92      |   |       |
| 25                   | UNIT B.403 |            | 50      |          |          |          |       |              |          |          |         |   |       |
| 26                   | UNIT B.404 |            | 51      |          |          |          |       |              |          |          |         |   |       |
| 27                   | UNIT B.501 | 5th FLOOR  |         |          |          |          |       |              |          |          |         |   |       |
| 28                   | UNIT B.502 |            |         |          |          |          |       |              |          |          |         |   |       |
| 29                   | UNIT B.503 |            |         |          |          |          |       |              |          |          |         |   |       |
| <b>GIA</b>           |            |            | 404     | 0        | 328      | 0        | 368   | 1100         | 0        | 0        | 0       | 0 | 0     |
| <b>No UNITS</b>      |            |            | 8       | 0        | 4        | 0        | 4     | 16           | 0        | 0        | 0       | 0 | 0     |

|                 |                       | 1 BED/2P   | 2BED/3P | 2 BED/4P | 3 BED/4P | 3 BED/5P |       | 1 BED/2P     | 2 BED/3P | 2 BED/4P | 3BED/5P |   |       |
|-----------------|-----------------------|------------|---------|----------|----------|----------|-------|--------------|----------|----------|---------|---|-------|
|                 |                       | UNIT AREAS |         |          |          |          |       | UNIT AREAS   |          |          |         |   |       |
| BLOCK C (NORTH) |                       | PRIVATE    |         |          |          |          | TOTAL | INTERMEDIATE |          |          |         |   | TOTAL |
|                 |                       | sqm        |         |          |          |          |       |              |          |          |         |   |       |
| 34              | UNIT C.001            | Grd FLOOR  |         |          |          |          |       |              |          |          |         |   |       |
| 35              | UNIT C.002            |            |         |          |          |          |       |              |          |          |         |   |       |
| 36              | UNIT C.101            | 1st FLOOR  |         |          | 71       |          |       |              |          |          |         |   |       |
| 37              | UNIT C.102            |            |         |          | 72       |          |       |              |          |          |         |   |       |
| 38              | UNIT C.103            |            | 52      |          |          |          |       |              |          |          |         |   |       |
| 39              | UNIT C.104 Wheelchair |            | 58      |          |          |          |       |              |          |          |         |   |       |
| 40              | UNIT C.201            | 2nd FLOOR  |         |          | 71       |          |       |              |          |          |         |   |       |
| 41              | UNIT C.202            |            |         |          | 72       |          |       |              |          |          |         |   |       |
| 42              | UNIT C.203            |            | 52      |          |          |          |       |              |          |          |         |   |       |
| 43              | UNIT C.204 Wheelchair |            | 58      |          |          |          |       |              |          |          |         |   |       |
| 44              | UNIT C.301            | 3rd FLOOR  |         |          | 71       |          |       |              |          |          |         |   |       |
| 45              | UNIT C.302            |            |         |          | 72       |          |       |              |          |          |         |   |       |
| 46              | UNIT C.303            |            | 52      |          |          |          |       |              |          |          |         |   |       |
| 47              | UNIT C.304 Wheelchair |            | 58      |          |          |          |       |              |          |          |         |   |       |
| 48              | UNIT C.401            | 4th FLOOR  |         |          | 71       |          |       |              |          |          |         |   |       |
| 49              | UNIT C.402            |            |         |          | 72       |          |       |              |          |          |         |   |       |
| 50              | UNIT C.403            |            | 52      |          |          |          |       |              |          |          |         |   |       |
| 51              | UNIT C.404 Wheelchair |            | 58      |          |          |          |       |              |          |          |         |   |       |
| 52              | UNIT C.501            | 5th FLOOR  |         |          | 71       |          |       |              |          |          |         |   |       |
| 53              | UNIT C.502            |            |         |          |          | 98       |       |              |          |          |         |   |       |
| <b>GIA</b>      |                       |            | 440     | 0        | 643      | 0        | 98    | 1181         | 0        | 0        | 0       | 0 | 0     |
| <b>No UNITS</b> |                       |            | 8       | 0        | 9        | 0        | 1     | 18           | 0        | 0        | 0       | 0 | 0     |

| TOTAL GIA APARTMENTS       |  | 844 | 0 | 1117 | 168 | 644 | sqm | 2773 | sqm | 29849 | sqft |
|----------------------------|--|-----|---|------|-----|-----|-----|------|-----|-------|------|
| <b>GIA AFFORDABLE RENT</b> |  | 0   | 0 | 146  | 168 | 0   | sqm | 314  | sqm | 3380  | sqft |
| <b>GIA SOCIAL RENT</b>     |  | 0   | 0 | 0    | 0   | 178 | sqm | 178  | sqm | 1916  | sqft |
| <b>GIA PRIVATE</b>         |  | 844 | 0 | 971  | 0   | 466 | sqm | 2281 | sqm | 24553 | sqft |

## Appendix 3: Recycling and refuse

CPG1 (2015) contains guidance on the requirements for recycling and refuse storage/ collection in new developments in Section 10.

This seeks to ensure that appropriate storage for recyclables and waste is provided in all developments in Camden. The guidance relates to Core Strategy Policy - CS18: Dealing with our waste and encouraging recycling and Development Plan Policies - DP26: Managing the impact of development on occupiers and neighbours and DP22: Promoting sustainable design and construction. It also relates to the British Standard BS5906-2005: Waste management in buildings – Code of practice. It provides detailed guidance on the space requirements for both internal and external storage features. It covers residential developments of up to 6 dwellings, of 7 dwellings and over and of non-residential or commercial units. The guidance applies to all new build developments;

The guidance explains that collection services for developments with 7 or more residential dwellings vary depending on the individual circumstances of the premises.

The scheme proposals include three refuse storage facilities as follows:

- 1 A refuse store for the affordable building situated on Georgiana Street (total 6 dwellings). Although not strictly required under CPG1, as it is for less than 7 new dwellings, the scheme includes a shared refuse store to allow for convenient storage and collection
- 2 A refuse store for the rest of the residential development located in the entrance passageway from Georgiana Street to the central courtyard (total 34 dwellings)
- 3 A separate refuse store for use by the B1 units located opposite (2) above (to serve a total of 868 sqm (GEA) B1)

The storage capacities of the first two of the above have been calculated in accordance with the guidance in CPG1 and the results are shown right.

1 Refuse storage for the affordable building:

| BLOCK A            | No Units | Litres Per Unit | TOTAL       |
|--------------------|----------|-----------------|-------------|
| 1 BEDS             | 0        | 100             | 0           |
| 2 BEDS             | 2        | 170             | 340         |
| 3 BEDS             | 4        | 240             | 960         |
| <b>TOTAL</b>       | <b>6</b> |                 | <b>1300</b> |
| No Bulk Bins Req'd |          |                 | <b>1.2</b>  |

This produces a requirement for storage of **1.2 bulk bins**

From CPG1 guidance, this means the refuse store for Block A should contain:

- 1 x 1100 litre Eurobin for general refuse
- 1 x 1100 litre Eurobin for dry recyclables
- 1 x 500 litre Eurobin for food waste

The refuse store has been designed to have room for the above.

2 Refuse storage for the rest of the residential development:

| BLOCK B&C          | No Units  | Litres Per Unit | TOTAL       |
|--------------------|-----------|-----------------|-------------|
| 1 BEDS             | 16        | 100             | 1600        |
| 2 BEDS             | 13        | 170             | 2210        |
| 3 BEDS             | 5         | 240             | 1200        |
| <b>TOTAL</b>       | <b>34</b> |                 | <b>5010</b> |
| No Bulk Bins Req'd |           |                 | <b>4.6</b>  |

This produces a requirement for storage of **4.6 bulk bins**

From CPG1 guidance, this means the refuse store for Blocks B and C should contain:

- 2 x 1100 litre Eurobin for general refuse
- 2 x 1100 litre Eurobin for dry recyclables
- 1 x 500 litre Eurobin for food waste

The refuse store has been designed to have room for the above.

3 Refuse storage for the B1 units:

There is no specific guidance on the amount of storage required for commercial purposes. A separate refuse store for commercial has been designed to house bins for general and recyclable refuse conveniently located in the building entrance way and easily accessible by the refuse collection team. It will house the following:

- 1 x 1100 litre Eurobin for general refuse
- 1 x 1100 litre Eurobin for dry recyclables



## Appendix 4: Designing out crime

The scheme has been designed with the principles of Secure by Design in mind.

During the preparation of the previous application, consultation took place with Adam Lindsey, Designing Out Crime Officer for the Camden Town area. Following a meeting that took place on 3 November 2015 to run through the proposals, PC Lindsey sent the following notes.

1. Doors. All communal and all residential doors will be to a security enhanced standard BS PAS 24-2012, LPS 1175 sr2, STS 201, STS 202 BR2, further information is available at [www.securedbydesign.com](http://www.securedbydesign.com)
2. Windows. All opening and accessible windows will be to BS PAS 24-2012 with P1A laminated glass.
3. Access control will be audio and video with no trades button fitted.
4. Post. Post delivered to a lobby with post boxes. Then a secondary BS PAS 24-2012 door fitted.
5. Utility meters in central location.
6. Stud partition. If fitted supported with 9mm plywood or expanded metal mesh.
7. CCTV. Registration and compliance with the information commissioner guidelines. [www.ico.gov.uk](http://www.ico.gov.uk)
8. Lighting. No bollard lighting.
9. Perimeter at this time is a 1.8m high close boarded fence.
10. Commercial unit may or may not be subject to SBD application.
11. Gating full height and width of opening, if secured with magnetic locks then they will be fitted one third from top and bottom.

All of the above were incorporated into the previous design and the same have been included in the current application.

## Appendix 5: Fire Brigade access

The scheme has been designed to provide safe access for fire fighters. Consultation took place with London Fire Brigade prior to the previous application and advice has been provided by an independent Fire Consultant.

Each of the three residential building cores will be fitted with dry risers in case of fire and the northern building will be fitted with sprinklers, if necessary, due to the distance from the roadway.

